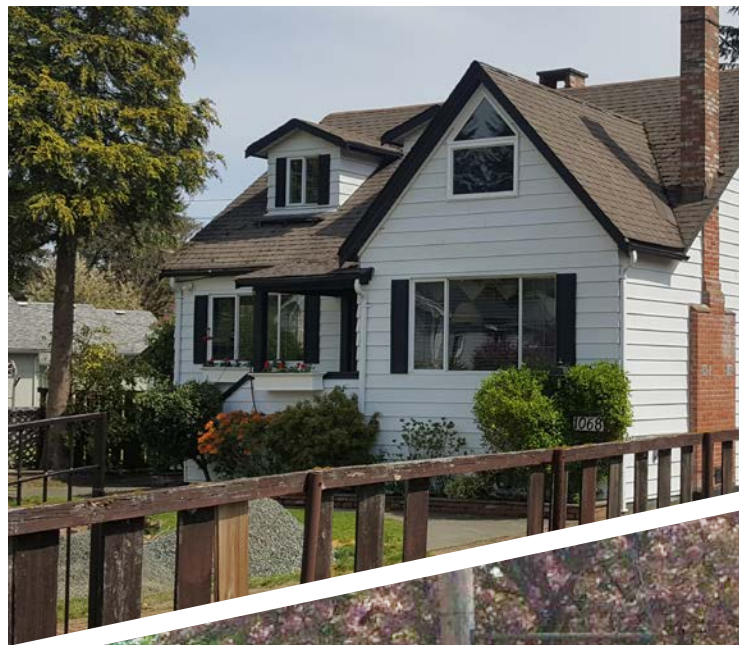


CAIRNSMORE

SUSTAINABLE NEIGHBOURHOOD PLAN



DRAFT FOR PUBLIC REVIEW
MAY 2020



The City of Duncan acknowledges that the land of the Cairnsmore neighbourhood is in the traditional, unceded territory of Cowichan Tribes, a Coast Salish people.



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PART A:

VISION & CONTEXT

I | VISION & KEY DIRECTIONS

A bright future starts with a strong vision. Together, the Cairnsmore neighbourhood has created a vision and key directions that guide the policies of the Cairnsmore Sustainable Neighbourhood Plan.

CAIRNSMORE VISION

In the future, Cairnsmore is a **close-knit community** where residents live, work, and play **through all stages of their lives**.

The Cairnsmore Neighbourhood Commercial Node is expanded to include more **local-scale businesses** intermixed with **established institutional uses**. Businesses are supported by high-quality **mixed housing** centered around **inclusive gathering destinations** and connected by **pedestrian and cycling routes**. The node attracts positive activity creating a **welcoming atmosphere**.

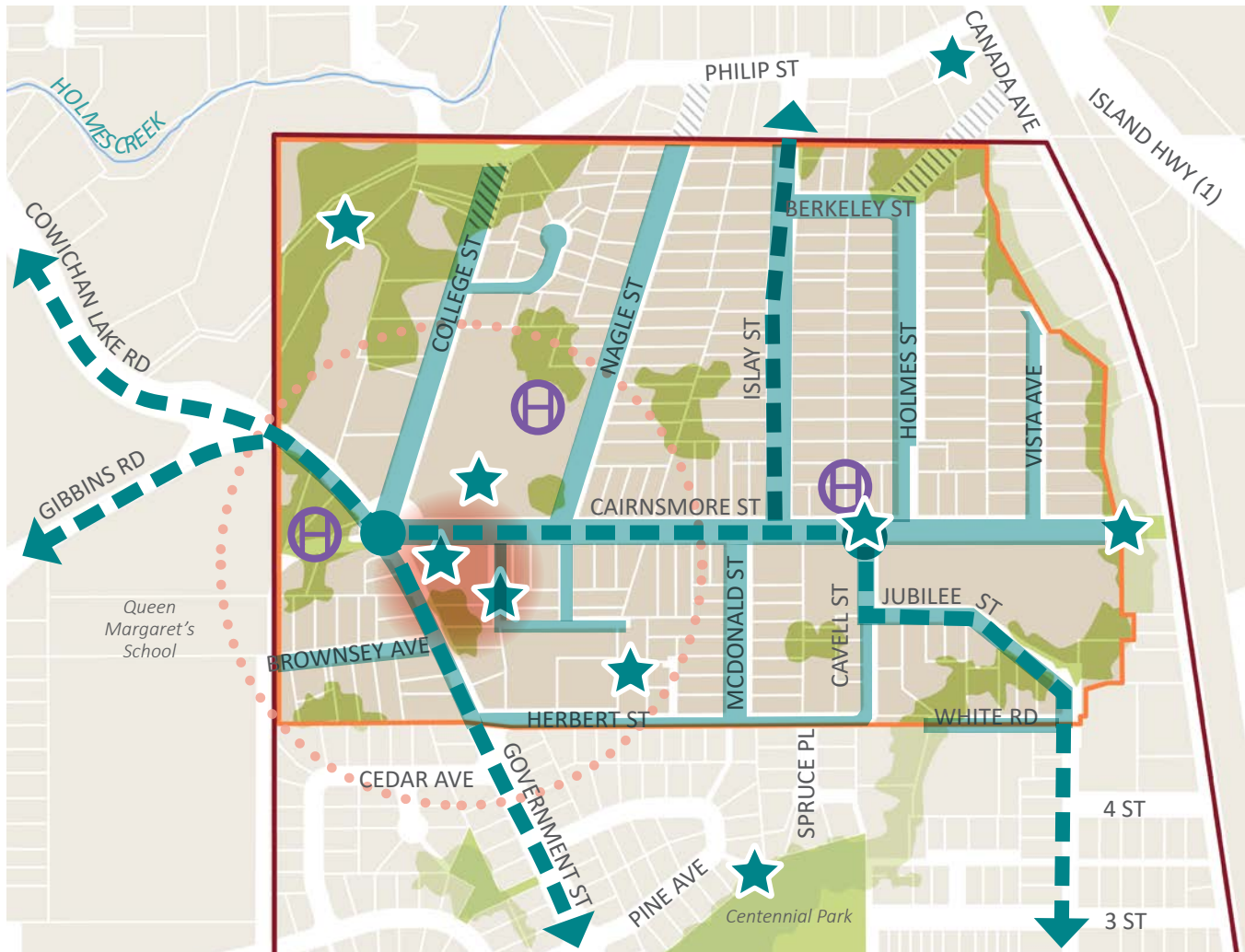
Mature trees and **large lots** continue to form the backdrop to unique **single-family character homes** that celebrate the area's early 1900s **architectural heritage**, with **new, appropriately-scaled housing options** artfully woven into the community fabric. **Sustainable infrastructure** supports growth and change.

In the future, Cairnsmore is an **affordable, safe, and healthy neighbourhood**.



KEY DIRECTIONS

FOR THE CAIRNSMORE NEIGHBOURHOOD



Map 1: Key Directions for the Cairnsmore Neighbourhood

-  A STRONG AND LOCAL NEIGHBOURHOOD COMMERCIAL NODE
-  MORE HOUSING DIVERSITY NEAR THE NEIGHBOURHOOD COMMERCIAL NODE
-  SENSITIVE RENEWAL AND INFILL THROUGHOUT THE NEIGHBOURHOOD
-  CELEBRATION OF HERITAGE AND CHARACTER
-  A CONNECTED COMMUNITY WITH MANY MOBILITY OPTIONS
-  BEAUTIFUL, FUNCTIONAL, COMPLETE STREETS FOR PEOPLE
-  RESPECT AND RENEWAL OF THE URBAN FOREST
-  A NEIGHBOURHOOD FOR PEOPLE
-  GREEN BUILDINGS
-  EFFICIENT AND UPDATED INFRASTRUCTURE



An active neighbourhood commercial centre



An attractive three-storey townhouse



A STRONG AND LOCAL NEIGHBOURHOOD COMMERCIAL NODE

What we heard from the community:

- The local grocer and coffee shop are valued
- Residents enjoy having nearby services
- Additional small-scale businesses would be welcome, as long as they fit the neighbourhood and do not compete with downtown
- Car-oriented or “franchise” commercial are not well supported
- Desire for the area to be more pedestrian-friendly
- Concerns about more activity increasing traffic at the Cairnsmore / Government intersection

How the plan addresses what we heard:

- Focuses all new commercial mixed use development within the neighbourhood commercial node
- Supports addition of local-scale commercial and limits car-oriented commercial
- Enhances sense of place and encourages positive activity through creation of public open spaces, ground-floor shops, and attractive streetscapes
- Prioritizes walking and cycling options along main streets
- Integrates surrounding and nearby institutional uses

To learn more:

- See Sections 4.4.1, 4.5



MORE HOUSING DIVERSITY NEAR THE NEIGHBOURHOOD COMMERCIAL NODE

What we heard from the community:

- Residents want to be able to remain in the neighbourhood through all life stages and need housing options that fit
- Concerns that large-scale apartments affect neighbourhood character and have contributed to past issues
- Scale and design quality are fundamental considerations for new multi-family housing

How the plan addresses what we heard:

- Encourages housing variety that supports all types of residents including older adults, young people, and lower-income individuals
- Identifies mixed use and medium density land uses near the neighbourhood commercial node and in areas closer to Downtown Duncan
- Encourages secondary suites within low density residential areas
- Identifies affordable housing units as a community amenity contribution
- Requires all new development or redevelopment to meet consistently high quality standards and be sensitive to the scale and character of the surrounding area, regardless of the income levels of the people who will live there

To learn more:

- See Sections 4.4.1, 4.4.2, 4.4.3, 4.5



Photo: Board and Vellu

An infill home sensitive to its context



Duncan Elementary / Cowichan Valley Open Learning Co-op

SENSITIVE RENEWAL AND INFILL THROUGHOUT THE NEIGHBOURHOOD

CELEBRATION OF HERITAGE AND CHARACTER

What we heard from the community:

- Owners take pride in maintaining attractive homes
- Larger lots with moderately-sized homes help protect mature trees
- New development should fit with the character of nearby homes
- Desire to maintain lower density character on local streets
- Uniqueness and variety is important
- Extensive subdivision is not desirable

How the plan addresses what we heard:

- Focuses higher densities in and around the neighbourhood commercial node, while maintaining lower densities in other areas
- Encourages secondary suites as a sensitive way to increase housing options in low density residential areas while maintaining existing character
- Encourages property owners to renovate, rather than demolish, existing homes
- Guides new development to respect the scale and form of neighbouring homes
- Encourages landscaped yards with permeable surfaces, gardens, and trees

To learn more:

- See Sections 4.4.4, 4.6, 4.8

What we heard from the community:

- First Nations heritage should be celebrated
- The architectural heritage of the area is valued
- Preservation of heritage buildings, sites, and features is important
- New developments should be complementary to heritage elements, yet flexible and modern in design
- Concerns about heritage requirements restricting an owners' ability to make changes to homes and sites

How the plan addresses what we heard:

- Encourages engagement with Cowichan Tribes to identify opportunities to integrate art, language, or other cultural elements into the planning and design process
- Encourages creation of a city-wide heritage framework to provide more certainty and support for preserving heritage architecture
- Supports redevelopment focused on retention and adaptive re-use of buildings with heritage elements over demolition and redevelopment
- Supports voluntary efforts for heritage identification and preservation

To learn more:

- See Sections 4.8, 7.4



Photo: Paul Krueger

A street with many transportation options



An attractive street focused on people



A CONNECTED COMMUNITY WITH MANY MOBILITY OPTIONS

What we heard from the community:

- Cairnsmore should be more pedestrian- and cycle-friendly
- Close proximity to Downtown Duncan and destinations like VIU, hospital, and schools make Cairnsmore a candidate for walkability
- Routes and schedules for transit to/from the neighbourhood are too limited to be convenient
- Concerns about negative activities, especially where passive surveillance is limited
- Congestion and speeding around the Government / Cairnsmore Street intersection is problematic
- Concerns that new development (both in Cairnsmore and in North Cowichan) will increase traffic problems and parking issues
- Parking is important, but should not dominate the landscape

How the plan addresses what we heard:

- Identifies priority active transportation routes
- Improves key intersections and street crossings to make walking more comfortable and safe
- Connects the neighbourhood to local, city, and regional destinations
- Recommends streetscape enhancements to increase comfort for all users

To learn more:

- See Sections 5.1, 5.2, 5.3, 5.4, 5.5

BEAUTIFUL, FUNCTIONAL, COMPLETE STREETS FOR PEOPLE

What we heard from the community:

- Streets should be pleasant, attractive, and safe for pedestrians, cyclists, and motorists
- More street trees and street furnishings including garbage receptacles and benches should be provided
- Traffic calming and pedestrian safety is a priority

How the plan addresses what we heard:

- Outlines an approach to Complete Streets that capitalize on the road right-of-way as attractive and usable public open space
- Addresses existing safety concerns and problematic intersections, including addition of a roundabout at the Cairnsmore St / Government St / College St intersection
- Requires new development and redevelopment to address the street and contribute to a pleasant pedestrian experience
- Prioritizes walkability in the neighbourhood commercial node
- Includes street tree planting throughout the neighbourhood

To learn more:

- See Sections 4.5, 4.6, 5.4, 5.5, 6.2



Existing mature tree backdrop in Cairnsmore



Kids playing in a park



RESPECT AND RENEWAL OF THE URBAN FOREST

What we heard from the community:

- Larger yards with smaller house footprints provide space for trees
- Large, mature trees are a character-defining element for Cairnsmore
- Concerns that large trees will be lost during changes to private lands, reducing the overall tree canopy in the neighbourhood
- More trees should be added to support a long-term mature tree canopy
- Fruit trees should be added to support food security

How the plan addresses what we heard:

- Suggests increasing measures for protecting mature trees on private lands
- Recommends a strong tree replacement strategy
- Grows the urban forest by adding new trees in public spaces, new development, and streetscapes
- Suggests education and incentives that encourage residents to voluntarily maintain existing and plant new trees on private property

To learn more:

- See Section 4.6.2, 6.2



A NEIGHBOURHOOD FOR PEOPLE

What we heard from the community:

- Desire for spaces and opportunities for the neighbourhood to come together
- Efficient use of existing social spaces
- Cairnsmore should be accessible and inclusive
- Desire for an increased feeling of safety
- Desire for neighbourhood involvement in decisions that affect Cairnsmore
- Create a neighbourhood identity
- Increase arts and culture presence, including thoughtful public art
- Support art-based businesses and tourism

How the plan addresses what we heard:

- Maximizes sharing and use of existing public and private facilities and open spaces
- Recommends locations for new parks and plazas
- Encourages architectural design that provides passive surveillance and feeling of safety
- Promotes CPTED and safe design principles
- Supports initiatives for a arts and culture
- Encourages resident-led initiatives and events
- Promotes an active neighbourhood network that supports and watches out for one another

To learn more:

- See Section 4.7, 5.5, 6.3, 7.2, 7.3, 7.4



Photo: La Citta Vita

Green townhouses



GREEN BUILDINGS

What we heard from the community:

- Home renovations should adapt existing homes to be more efficient
- Encourage new development to achieve high sustainability standards (e.g. solar, low impact development, recycling, reuse, green roof)

How the plan addresses what we heard:

- Sets maximum impervious areas for sites to reduce run-off and support natural groundwater recharge
- Requires new development to meet a minimum Step 3 of the BC Energy Step Code
- Encourages building design that considers passive heating, cooling, and lighting design principles
- Requires new buildings to be designed in a way that supports adaptation to future sustainable technologies

To learn more:

- See Section 4.7



A stormwater swale in a downtown street



EFFICIENT AND UPDATED INFRASTRUCTURE

What we heard from the community:

- Stormwater issues are important
- Support for sustainable infrastructure
- New development should not impact delivery of services to existing homes

How the plan addresses what we heard:

- Provides land use information so that roads, utilities, sewer, and water can be planned to meet community needs even as the neighbourhood grows
- Requires new development to provide appropriate investment to the infrastructure network
- Requires new development to use efficient design that helps reduce rainwater runoff, water consumption, waste generation, and energy consumption that increase pressures on infrastructure networks
- Integrates stormwater management with streetscape upgrades
- Requires LED lights in all new street or decorative lighting installations

To learn more:

- See Sections 4.3, 4.7, 8.2, 8.3, 8.4

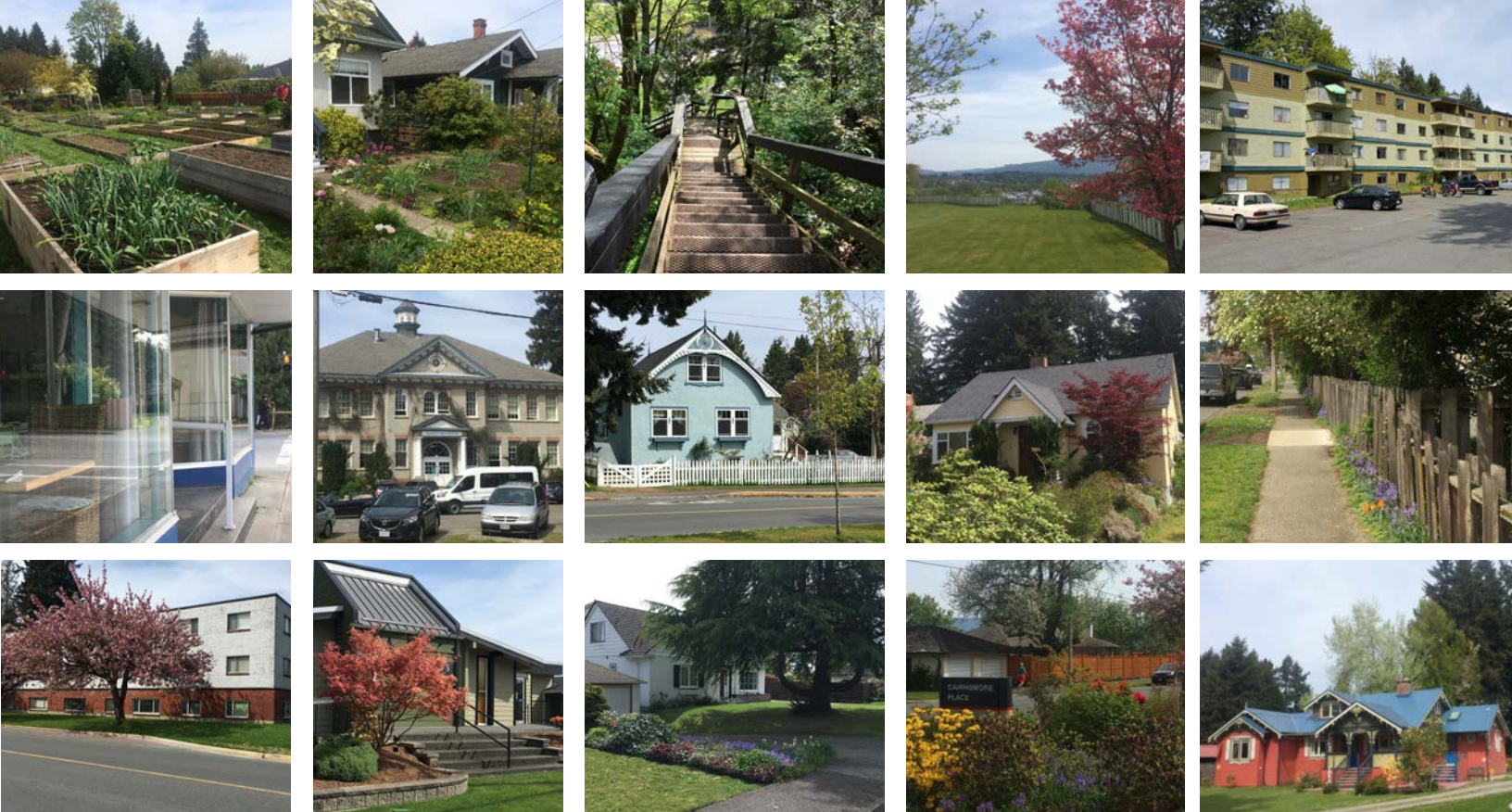


Figure 1: Visual Sample of the Diversity and Character that Make Cairnsmore Unique

2 | ABOUT THIS PROCESS

There is much that people love about Cairnsmore today – established character, mature trees, proximity to the downtown and hospital. The City of Duncan created this neighbourhood plan for Cairnsmore to guide future changes to protect the neighbourhood's existing assets and implement residents' vision for the future.

In 2018, the City of Duncan initiated the Cairnsmore Sustainable Neighbourhood Plan, working with community members to decide what is important to protect and celebrate in Cairnsmore, and to identify potential improvements and prepare for how the neighbourhood could evolve. Residents, business owners, community groups, and City staff have been involved in shaping this plan.

The goal of the Cairnsmore Sustainable Neighbourhood Plan is to help maintain existing assets while planning

how future development and change should be integrated over the coming decades. The resulting plan provides a guiding vision for Cairnsmore along with key directions and policies to support realization of the vision over time.

A neighbourhood vision is achieved incrementally and over time through the combined efforts of residents, developers, City staff, and elected officials making thoughtful decisions that move the neighbourhood towards its vision for the future.

2.1 WHAT IS A NEIGHBOURHOOD PLAN?

As much as we would like our neighbourhoods to stay the same, change is inevitable: neighbours move, businesses change, buildings and infrastructure age.


The City's Official Community Plan (OCP) and Integrated Community Sustainability Plan (ICSP) provide high-level guidance for how the City of Duncan will grow and evolve. A neighbourhood plan builds upon this guidance at a neighbourhood level, with specific key directions and policy focused on the neighbourhood's needs.

The plan addresses questions like:

- What **types of housing** should be available in Cairnsmore and where should they go?
- What type of **design** and **green building elements** should be incorporated in new buildings?
- How will people in Cairnsmore **get around** the neighbourhood and to other parts of the community?
- How should **green spaces** and the **environment** be protected and improved?
- What **services** and **resources** will future residents and businesses need?



Figure 2: Neighbourhood Plan Overview



Example of a green and attractive neighbourhood commercial node. A neighbourhood plan helps guide changes that work towards an attractive, sustainable future.

WHO BENEFITS FROM A NEIGHBOURHOOD PLAN?

RESIDENTS participate in the plan development to identify issues important to them and provide input to changes that will direct desired change. They can use the plan to encourage changes that align with the vision that's been created. Many aspects of the plan can occur through volunteerism or neighbourhood initiative, with residents supporting its success.

DEVELOPERS have a better understanding of what residents want to see built in Cairnsmore. When developers understand neighbourhood concerns and objectives, they are better able to develop projects that will be supported by local residents and work towards the neighbourhood's vision.

CITY STAFF also have a better understanding of how the neighbourhood would like to accommodate growth and what elements of their neighbourhood residents would like to see protected or enhanced. Staff are able to identify city-led projects that will support progress towards the vision.

CITY COUNCIL is better able to make decisions that will benefit the neighbourhood when it understands local residents' expectations.

It is important to realize that the vision and key directions of this plan will not become reality overnight. The implementation of a neighbourhood plan relies heavily on local residents as well as the local economy. For the neighbourhood plan to work, all groups must work together.

Photo: SDOT

2.2 HOW TO USE THIS PLAN

The Cairnsmore Sustainable Neighbourhood Plan is a long-range planning document that promotes a vision, policies, and actions that will be implemented through three pathways:

- **Development driven**, through rezoning, development permit, and subdivision applications. Each private project will be reviewed against the guidance and policies of the Cairnsmore Sustainable Neighbourhood Plan to assess its ability to help achieve the plan’s vision and key directions.
- **Neighbourhood driven**, through volunteerism and community participation. Residents can use the plan to identify social or community projects or activities that enhance the neighbourhood.
- **Community driven**, through Council decision. The neighbourhood plan will be used during project planning and budget deliberations to help determine public investments in the area.

This is a planning-level document and boundaries and locations of symbols and areas shown are approximate only. Where quantities or numerical standards are shown, these shall be interpreted as conceptual only, to be defined at detailed design stages. The policies and guidelines in this plan are not to be interpreted as an approval for a use on a specific site. It should be noted that no representation is made herein that any particular site is suitable for a particular purpose, as the site conditions or limitations must be assessed on a case-by-case basis.

2.3 RELATIONSHIP TO OTHER PLANS

The Cairnsmore Sustainable Neighbourhood Plan is intended to apply and focus community planning and policy at the neighbourhood level.

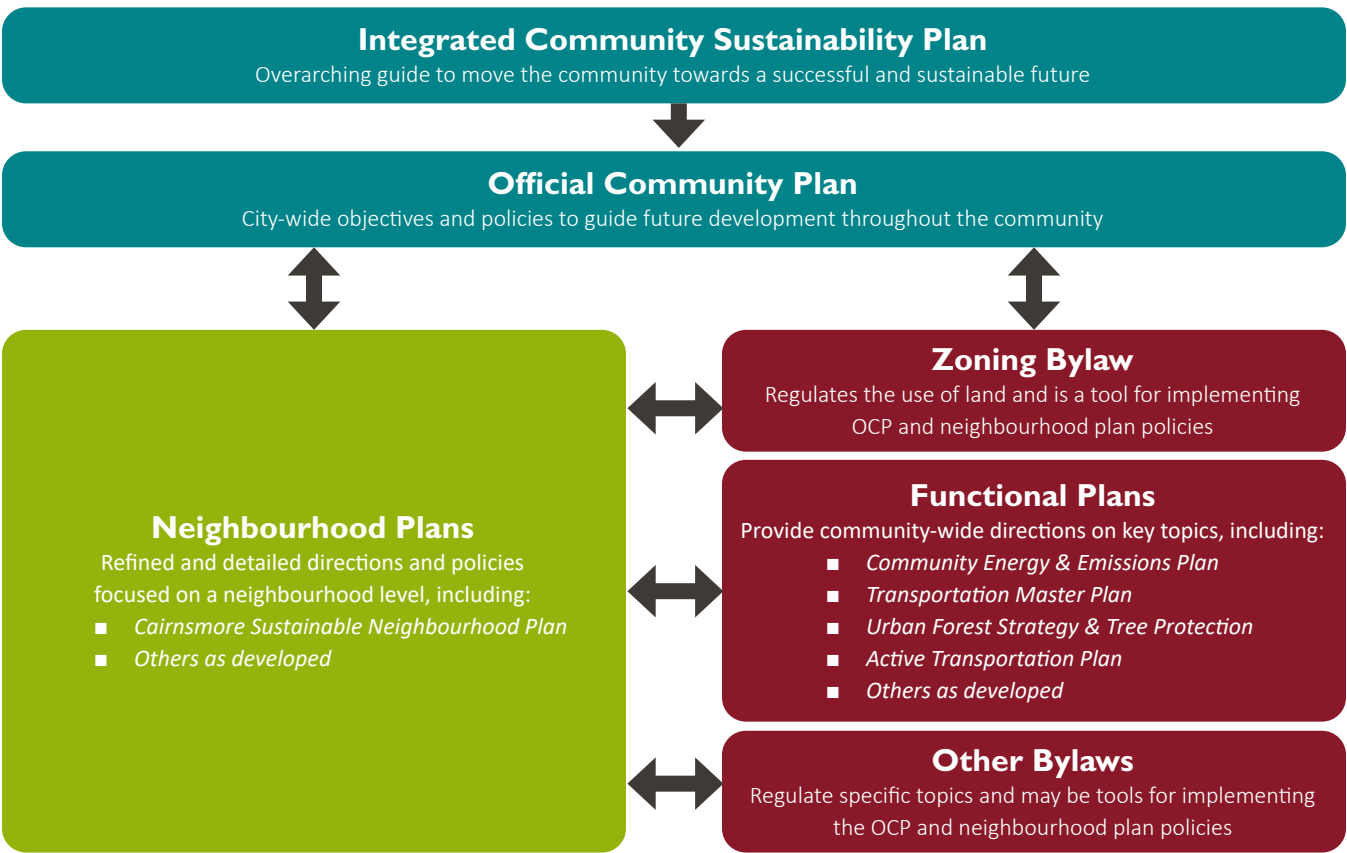


Figure 3: Relationship Between Plans

The Cairnsmore Sustainable Neighbourhood Plan is intended to align with current guiding City of Duncan planning documents. Where it does not align, updates are noted in the implementation plan.

INTEGRATED COMMUNITY SUSTAINABILITY PLAN (2013)

The ICSP guides Duncan towards a sustainable future. The ICSP's priorities for success include moving towards a carbon neutral community, zero-waste and more active transportation options; a healthy, active community for people at all life stages; protection of natural resources, food sources, and water supply; and an affordable community with a diverse economy.

OFFICIAL COMMUNITY PLAN (2007)

Duncan's OCP defines a vision "to be one of the most livable small towns in Canada." Key themes that affect the Cairnsmore Sustainable Neighbourhood Plan include:

- Support for Smart Growth principles
- Future development focused in walkable, compact neighbourhoods
- New development that is a "good fit" with the character of an existing neighbourhood
- Mixed use and multi-family land uses in the Cairnsmore neighbourhood commercial node
- A range of inclusive housing types
- A thriving economy
- Promotion of active transportation
- Distinct identities for neighbourhood nodes
- Compatible commercial activities in neighbourhoods
- Stewardship of the environment and trees
- Climate protection, reduction of energy and water consumption, emissions reduction
- Celebration of arts and culture

COMMUNITY ENERGY & EMISSIONS PLAN (2015)

Duncan's CEEP models the City's current energy use and greenhouse gas emissions and tracks them against Duncan's target of an 80% reduction in emissions by 2050. An action plan provides concrete steps to help reduce energy consumption and emissions. Aligning the Cairnsmore Sustainable Neighbourhood Plan with CEEP actions will help support progress towards the goal.

ACTIVE TRANSPORTATION PLAN (2014)

The Active Transportation Plan is intended to guide well-designed and integrated walking and cycling infrastructure in Duncan. The plan recognizes Government Street, Jubilee Street, and Cairnsmore Street as cycling and pedestrian connections and recommends upgrades to improve conditions.

TRANSPORTATION MASTER PLAN (UNDERWAY)

The Transportation Master Plan (currently under development) will provide guidance to support the City's aspirations for sustainable growth patterns and help expand travel choices within Duncan. The Cairnsmore Sustainable Neighbourhood Plan provides direction on desired conditions for streets within Cairnsmore and it is anticipated that the intent outlined in this plan will be incorporated and refined in the Transportation Master Plan.

URBAN FOREST STRATEGY (2010) & TREE PROTECTION BYLAW (2015)

The Urban Forest Strategy provides a vision and plan for long term sustainable urban forest management. From the plan, a Tree Protection Bylaw has been adopted to regulate the removal of private trees and preserve the overall ecological function of the urban forest.

2.4 PLANNING PROCESS

The Cairnsmore Sustainable Neighbourhood Plan was initiated in Spring 2018 and was organized around three key phases:

- **PHASE 1** focused on building an understanding of participants' perceptions of the current Cairnsmore neighbourhood – what they love, what concerns them, and what their hopes are for the future – and an analysis of existing neighbourhood conditions including demographics, land use, transportation, infrastructure, amenities, and character.
- **PHASE 2** developed a draft vision and key directions, along with a number of emerging policy directions being considered. The community reviewed the ideas, providing input to confirm those to be advanced in the neighbourhood plan and identifying additional ideas that may warrant consideration.
- **PHASE 3** developed and refined the neighbourhood plan and consideration of the draft plan by City Council.

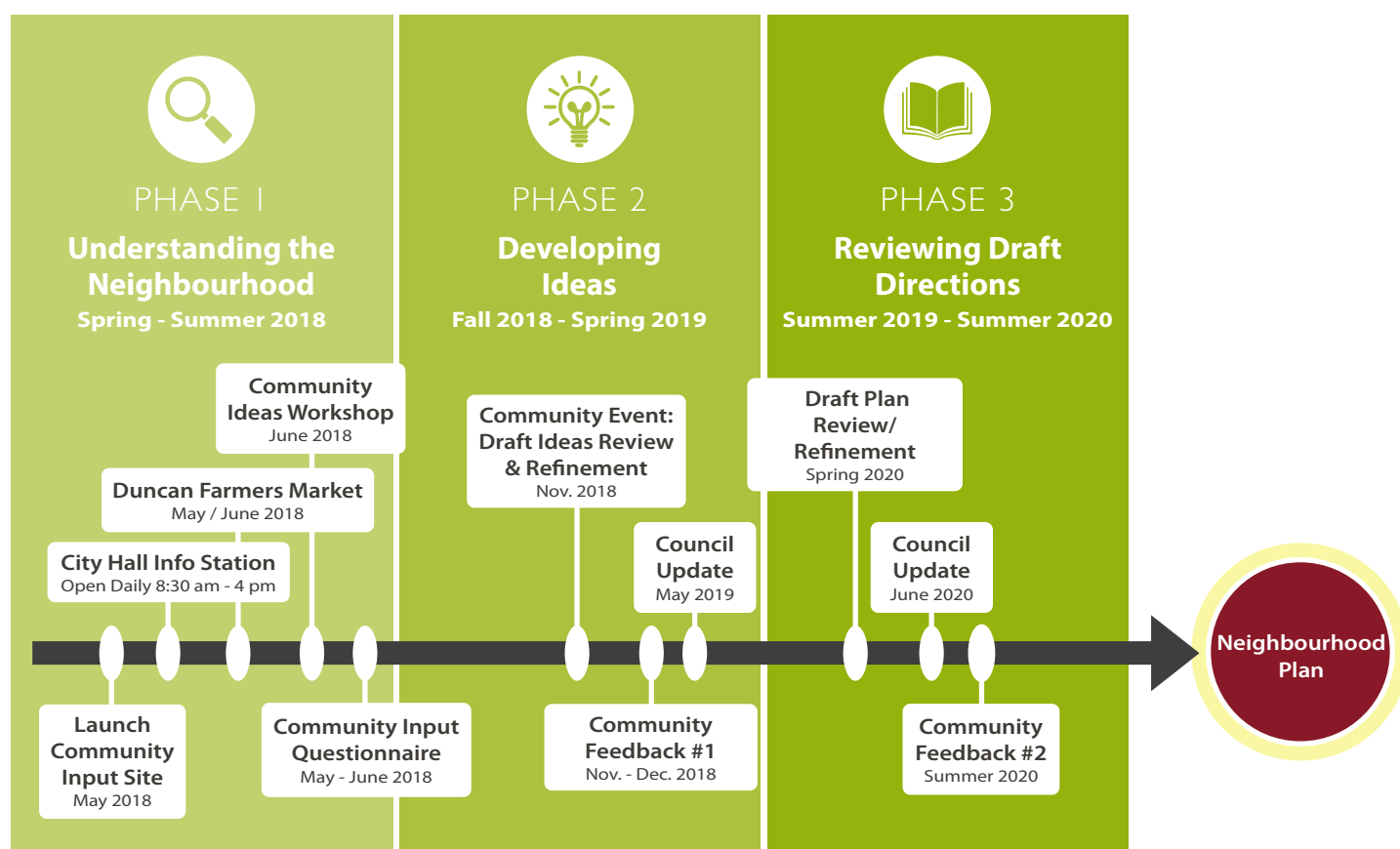


Figure 4: Planning Process Diagram



Herbert Street in Cairnsmore

3 | NEIGHBOURHOOD CONTEXT

The Cairnsmore neighbourhood is unique. Analyzing baseline information about existing features and neighbourhood structure helps inform community discussions and guide planning directions.

While the City of Duncan and surrounding areas have seen change in recent years, change in Cairnsmore has been relatively slow: some homes have been renovated or replaced, minor subdivision has occurred, businesses have changed, and building uses, like Duncan Elementary, have evolved. However, the general neighbourhood structure has been largely constant since the 1950s.

An outcome of this slow pace of change has been the preservation of a unique neighbourhood character formed by varied homes and lot sizes, mature trees, and a mix of long-time and new residents.

However, change is on the horizon.

- Pace of new development in the City and region has been increasing;
- A new hospice adjacent to Cairnsmore Place is being developed;
- The nearby Cowichan District Hospital will be relocated to a new site in the coming years;
- Some buildings are aging making them candidates for renewal or replacement; and
- Growth has been rapid in adjacent Municipality of North Cowichan neighbourhoods to the north and west, increasing traffic on streets in Cairnsmore.

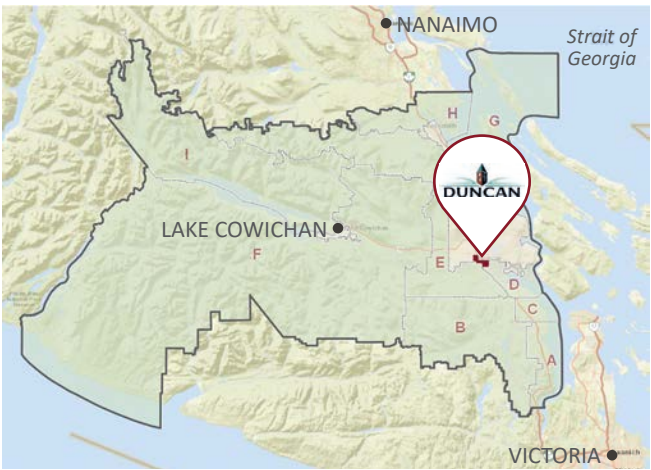
3.1 PLAN AREA

Cairnsmore is the northern-most of Duncan's six neighbourhoods, situated between the Municipality of North Cowichan to the north and west and Duncan's Centennial Heights neighbourhood to the south and Downtown neighbourhood to the southeast.

The neighbourhood perches above a bank that forms the east neighbourhood boundary. The topography limits access to the area, with main entry connections via Philip Street to the north, Lake Cowichan Road / Gibbins Road / Government Street to the south and west, and Jubilee Street to the southeast.

The rich inventory of heritage and character homes on larger lots; its proximity to employment centres, including Cowichan District Hospital, Vancouver Island University, and Downtown Duncan; and its collection of institutional uses and essential local businesses make Cairnsmore a desirable neighbourhood to call home.

Map 3: Regional Context



The City of Duncan, incorporated in 1912, is an economic and civic hub of the Cowichan Valley, located midway between the cities of Victoria and Nanaimo. The City has a population of approximately 5,000 who live within a small land area of 2.07 km². Most of the land in Duncan has been developed, making it a more urban setting than the surrounding Cowichan Valley.

Map 2: Neighbourhood Location



3.2 REGIONAL TRENDS

In 2017, the communities of the Cowichan Valley Regional District, including City of Duncan, initiated the Cowichan 2050 Regional Collaboration Framework to support more “sustainable and coordinated growth and development in the region.” The **Cowichan 2050 Background** outlines several important regional trends that are relevant to planning for communities and neighbourhoods:

- **Future Population Growth** – While the exact rate of growth is uncertain, it is assumed the population of the CVRD could grow by 30% over the next 30 years. This would mean an additional 25,000 people looking to call the region home (the equivalent of 5 new Duncans) in addition to the 83,000 that currently live in the CVRD.
What this means for Cairnsmore: All communities and neighbourhoods will need to think about how to absorb some growth. Integrating smart growth in Cairnsmore, while maintaining neighbourhood character, will be important.
- **Green House Gas Emissions** – 90% of travel in the CVRD is by personal vehicle and vehicle emissions are the main contributor to greenhouse gases in the region (72% versus 58% provincially).
What this means for Cairnsmore: Opportunities exist in Cairnsmore to reduce automobile dependency by improving multi-modal connectivity within the neighbourhood and to adjacent destinations like Downtown Duncan and Cowichan Hospital and by increasing local services and housing near the neighbourhood commercial node that mean people can live close to what they need.
- **Housing Security** – The region’s income gap is growing, housing insecurity is on the rise, and vulnerable populations face systemic barriers such as poverty, difficulty accessing employment, and pay gaps. Housing that is affordable can help keep people from sliding into financial hardship.
What this means for Cairnsmore: It will be important to find ways to improve existing affordable homes and integrate new affordable spaces in the neighbourhood in a way that is equitable and respectful.
- **Land Conservation** – Lands already impacted by forestry and development account for about 75% of the CVRD's total land area.
What this means for Cairnsmore: Focusing new urban growth within established neighbourhoods such as Cairnsmore will be a regional priority, in order to limit further greenfield development that impacts remaining undeveloped lands.
- **Heritage** – The physical, cultural, and social heritage resources in the CVRD are considered defining features for the region.
What this means for Cairnsmore: Support for protecting and celebrating heritage and character assets is high and opportunities may exist to collaborate on preservation and acknowledgment.
- **Climate Change** – A warmer, drier climate with more extremes is predicted for the CVRD potentially impacting water supplies, agriculture, and forests.
What this means for Cairnsmore: At the neighbourhood level, preparing for water conservation, reducing waste, and encouraging alternate modes of transportation are important to reducing our environmental footprint. Changing climate may also impact future regeneration of mature trees, meaning it could become increasingly difficult to replace Cairnsmore's mature trees once removed.
- **Food Security** – 14% of Cowichan residents face food insecurity, impacting low-income households and food prices are rising.
What this means for Cairnsmore: Local food production opportunities at community spaces and within private gardens provides potential opportunities to support healthy food production.

In addition to regional trends, the emergence of COVID-19 in 2020 is anticipated to have far-reaching impacts from the global down to the neighbourhood levels. While the full range of impacts are unknown at this time, it should be anticipated that communities and neighbourhoods will need to adapt to emerging planning trends resulting from this change.

3.3 EXISTING URBAN STRUCTURE & LAND USE

Many Cairnsmore’s residential areas are characterized by larger than average lots with moderately-sized single-family homes. A few multi-family buildings are located along Government Street and Cairnsmore Street within medium density residential areas.

While Cairnsmore is primarily residential, it also has several institutional centres including Cairnsmore Place, Duncan Elementary (Cowichan Valley Open Learning Co-op), the former Duncan Primary, Queen Margaret’s School, St. Andrews Presbyterian Church, daycares, and seniors living facilities. A small neighbourhood commercial node at the corner of Cairnsmore and Government Streets is an anchor with local grocery, a coffee shop, and a service station.

Outside Cairnsmore, residential areas follow a mostly suburban layout with mainly single-family homes on moderately sized lots and a mix of institutional (hospital, school), multi-family, commercial, and some larger residential lots.

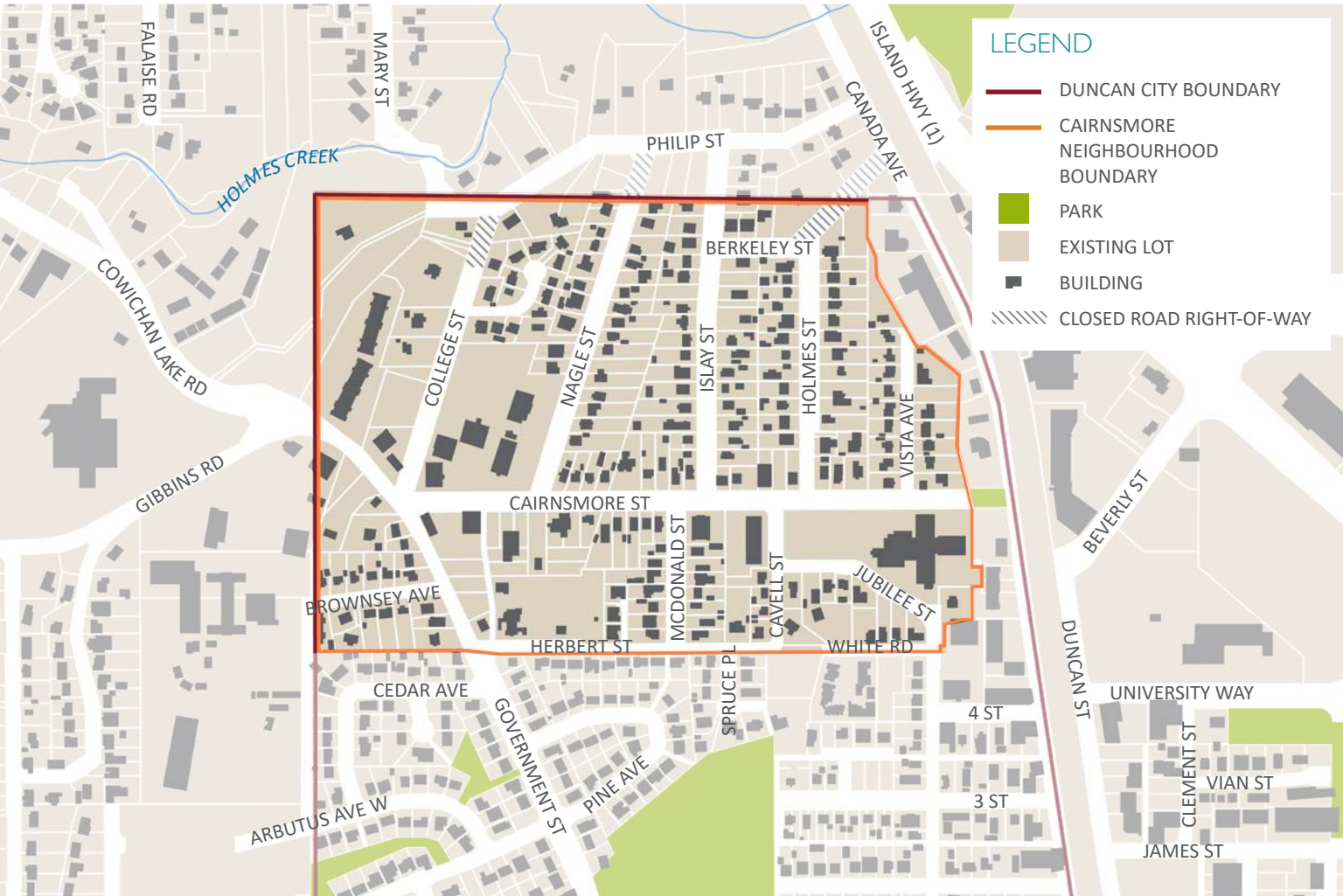


Typical low density residential street in Cairnsmore.



The 100-unit Gala Vista apartment is the largest-scale medium density residential development in Cairnsmore.

Map 4: Figure-ground of Cairnsmore and Surrounding Area



3.4 NEIGHBOURHOOD CHARACTER

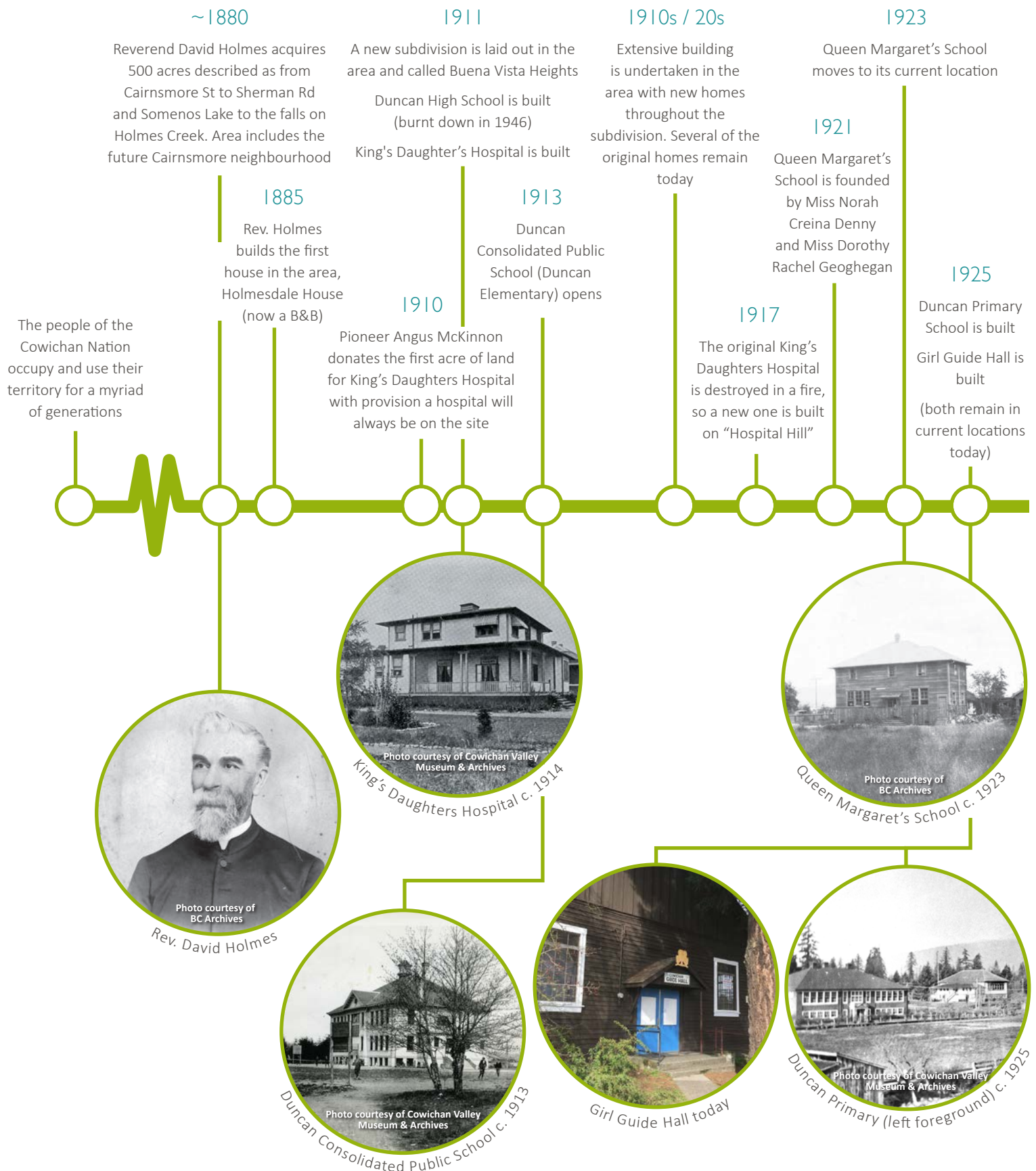
What makes Cairnsmore special? Through engagement, community members identified the character-defining features shown in Figure 5. These features help determine policies and guidelines for the neighbourhood plan that build on these character features.

Figure 5: Character-defining Cairnsmore Features



3.5 NEIGHBOURHOOD HISTORY & EVOLUTION

Figure 6: Notable Points in Cairnsmore's Recorded History



1960

St. Andrews Presbyterian Church relocates from First St to its current location in Cairnsmore

1996

Fire damages Cairnsmore Place, with over 200 neighbours coming together to help safely evacuate residents. The facility is restored

2016

The Fishbowl Cafe opens creating a neighbourhood meeting place

2015

Cowichan Valley Open Learning Co-op starts classes at the Duncan Elementary school site

1972

Cairnsmore Place is opened on the former King's Daughters Hospital site to provide residential facilities for seniors and people needing long-term care

2008

St. Andrews Redemption Garden is established to provide community gardens for the church and neighbours

2013

The School District stops using Duncan Elementary for elementary classes

1933

The Chapel at Queen Margaret's School is built

1967

King's Daughters Hospital is closed and demolished after 50+ years of service



Photo courtesy of Cowichan Valley Museum & Archives

King's Daughters Hospital c. 1948



St. Andrews Redemption Gardens today



Photo courtesy of QMS

Queen Margaret's Chapel today



Cairnsmore Place today



St. Andrews Presbyterian Church today



Fishbowl Cafe today

CAIRNSMORE IN THE EARLY 1900s

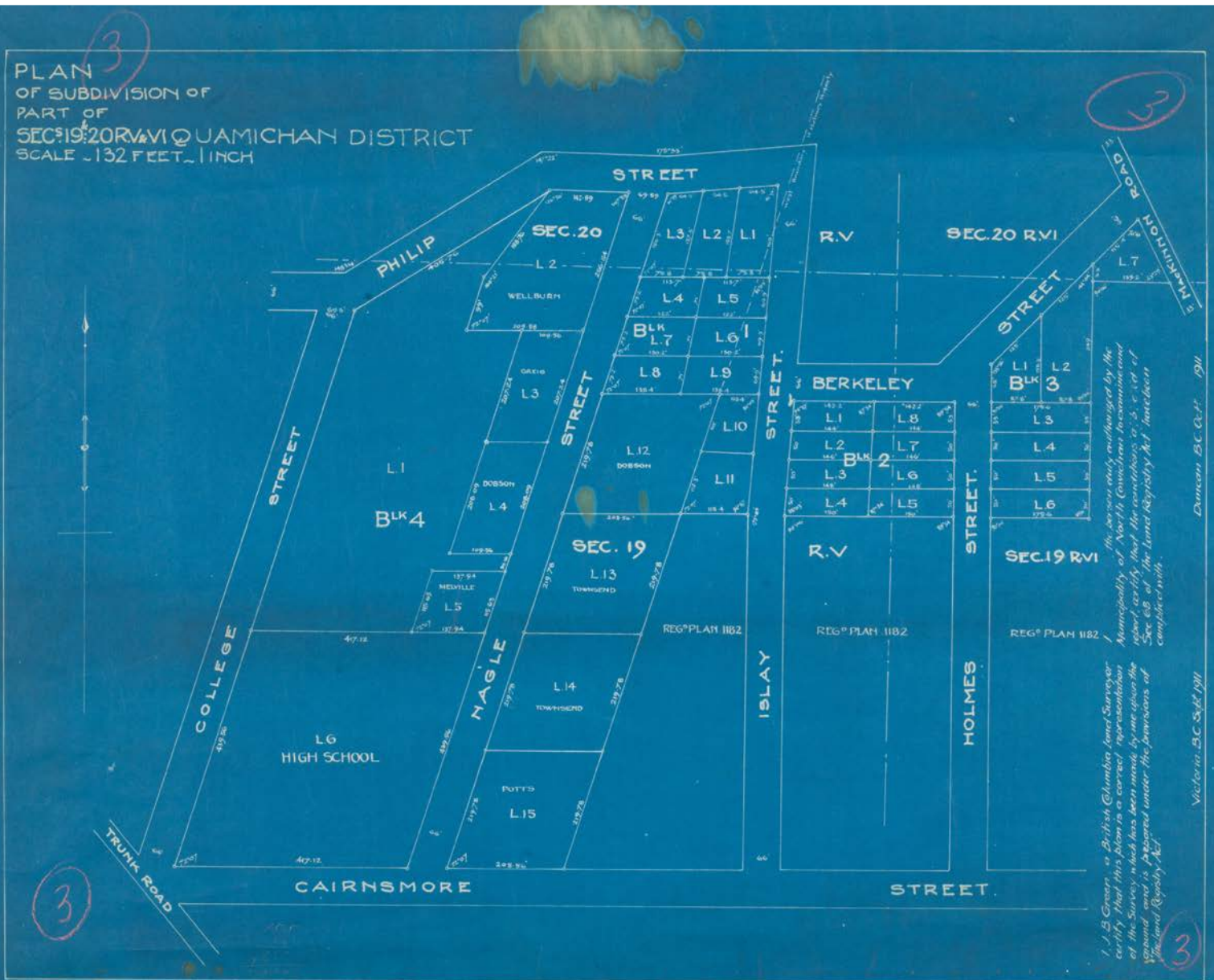


Figure 6: 1911 Subdivision Plan for the Cairnsmore Neighbourhood
Plan courtesy of the Royal BC Museum



Figure 7: View looking towards Cairnsmore Neighbourhood, c. 1912
Photo courtesy of Cowichan Valley Museum Archives



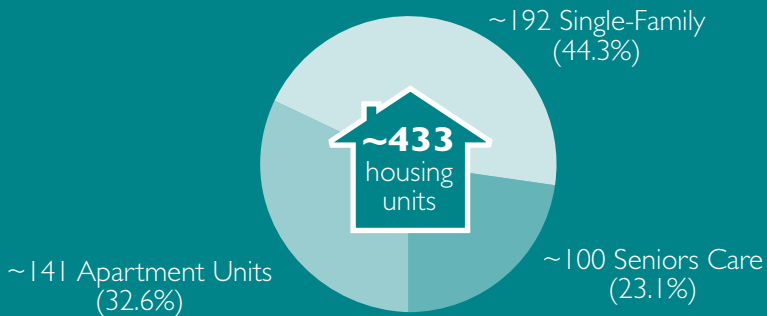
Figure 8: View of Schools in Cairnsmore, c. 1925
Photo courtesy of Cowichan Valley Museum Archives

3.6 NEIGHBOURHOOD SNAPSHOT

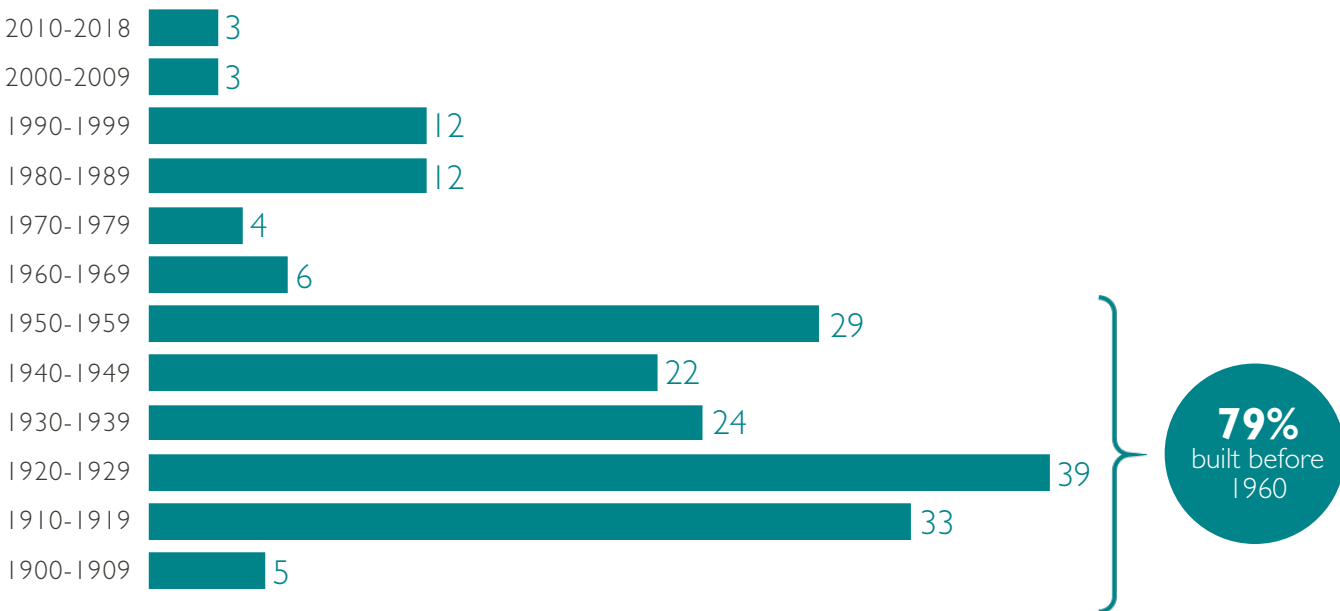
POPULATION*



EXISTING HOUSING TYPES**



AGE OF SINGLE-FAMILY HOUSING STOCK**



OVERVIEW OF MULTI-FAMILY HOUSING STOCK**

Name	Location	Year Built	# Units	# Floors	Type
Fishbowl Building (2nd Floor)	550 Cairnsmore St	1955	2	2	Rental
Springridge Manor	961 Cavell St	1964	31	3	Rental
Gala Vista Apartments	1020 Government St	1975	100	4	Rental
Herbert St	461-465 Herbert St	1989	3	1	Strata
370 Cairnsmore	370 Cairnsmore St	1992	8	2	Strata

HOUSEHOLD STATS*

MEDIAN AGE



AVERAGE HOUSEHOLD SIZE



% HOUSEHOLDS WITH CHILDREN AT HOME



POPULATION DENSITY (PERSONS/HA)



MEDIAN HOUSEHOLD INCOME



% OF LOW INCOME HOUSEHOLDS



% OF PROPERTY OWNERS SPENDING 30% OR MORE INCOME ON HOUSING



% OF RENTERS SPENDING 30% OR MORE INCOME ON HOUSING



OWNERS & RENTERS*

CAIRNSMORE



DUNCAN



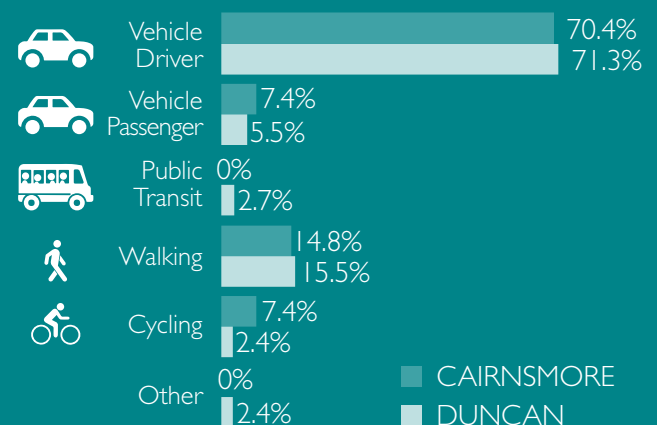
Sources:

* Statistics Canada 2016 Census, Dissemination Area 59190240.

Note: Census boundaries do not align precisely with neighbourhood boundaries. Excluded areas could lead to minor variations in statistics.

** BC Assessment, 2018 data.

PRIMARY MODE OF TRANSPORTATION TO/FROM EMPLOYMENT*



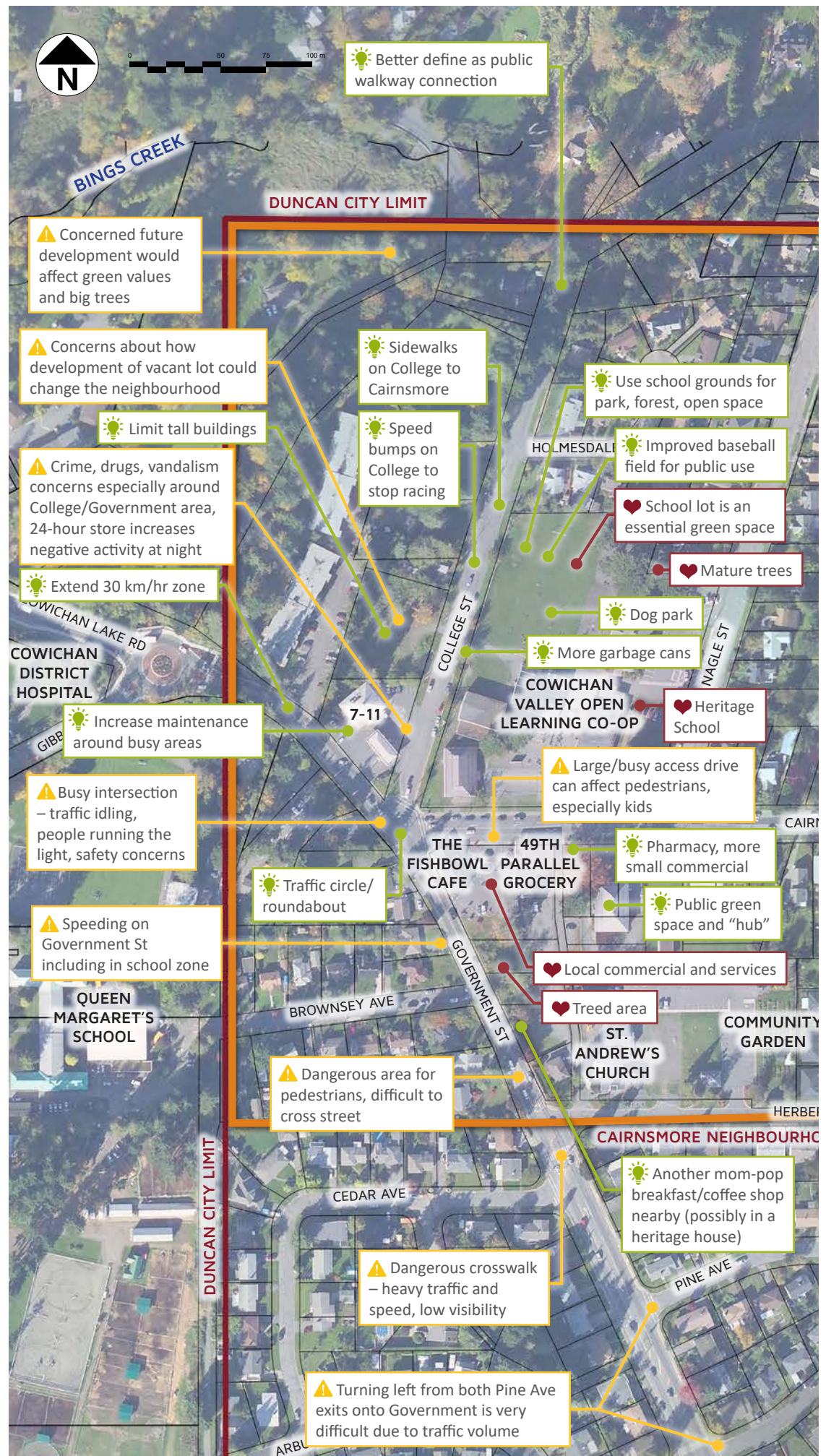
3.7 VALUES, CONCERNS & IDEAS

Throughout the process, participants recorded important places, observations, and ideas about Cairnsmore. The map summarizes observations and ideas that were considered during development of the neighbourhood plan.

Map 5:
Neighborhood-Identified
Values, Concerns, and Ideas ►

LEGEND

- ♥ **VALUES:** What people love about Cairnsmore
- ⚠ **CONCERNS:** What people think could be improved
- 💡 **IDEAS:** What people might like to see in the future



COMMUNITY VALUES, CONCERNS & IDEAS



SUSTAINABLE NEIGHBOURHOOD PLAN

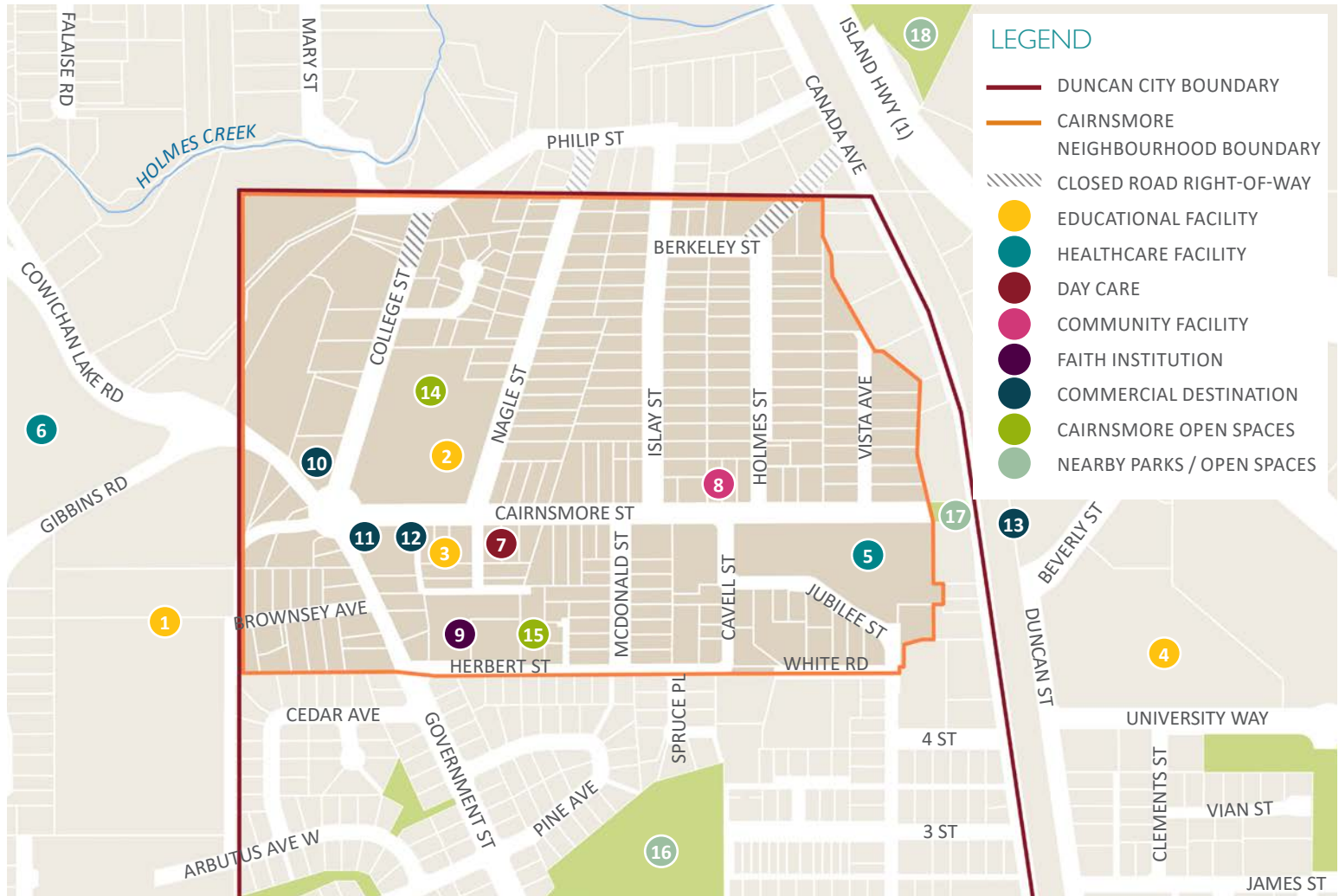
Your Cairnsmore. Your Plan. Let's Talk!



3.8 PLACES OF INTEREST

Cairnsmore is home to several places of interest for people living in the neighbourhood, elsewhere in the City, and beyond. Map 6 shows a selection of key locations and places of interest in and near Cairnsmore.

Map 6: Places of Interest



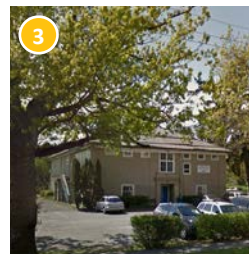
1 Queen Margaret's School

provides a junior school program for boys and girls (K-7), a senior school for girls (8-12), and a preschool and junior kindergarten program.



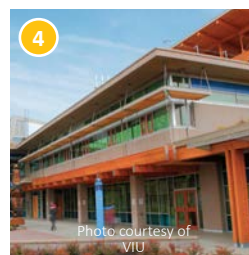
2 Cowichan Valley Open Learning Co-op (CVOLC)

occupies the Duncan Elementary building and offers alternative programs to students in School District 79 that are personalized and flexible to meet students' needs.



3

Duncan Primary, located in Cairnsmore's neighbourhood commercial node and owned by School District 79, formerly provided educational services.



4

VIU Cowichan Campus, with an enrollment of over 1,500 students, offers a range of programs. Cairnsmore's proximity makes the neighbourhood potentially attractive to students and staff.



Cairnsmore Place is a 100-unit long-term care facility providing living spaces and amenities for people requiring supportive care.



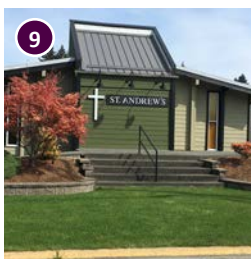
Cowichan District Hospital, opened in 1967, has 134 beds. The hospital will be moved to a new location in the next 5-10 years. Future use of the hospital site is unknown.



Growing Together Child and Parent Society provides child care and early learning to children of faculty and students at CVOLC as well as neighbourhood residents.



Girl Guide Hall, owned and operated by the Girl Guides of Canada, provides community hall facilities.



St. Andrews Presbyterian Church is located on Herbert Street. In addition to worship services, the church offers a variety of programs for adults, youth, and children.



Petro Canada and 7-11 are located at the Government / Cairnsmore intersection. Open 24 hrs, the station provides gas and convenience services.



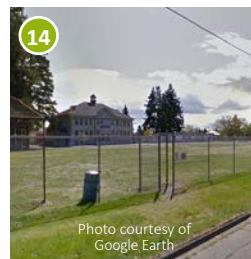
The Fishbowl Cafe, coffee shop and restaurant, has become a common gathering place for local residents.



The 49th Parallel Grocery Store, is a small, locally-oriented grocery store within walking distance for most Cairnsmore residents.



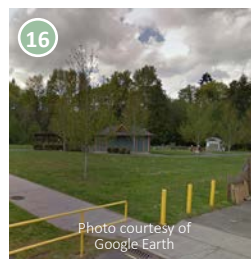
Canada Ave and Beverly Corners, to the east provide many retail services. While close to Cairnsmore, barriers including slopes, roads, rail line, and lack of pedestrian facilities can limit access.



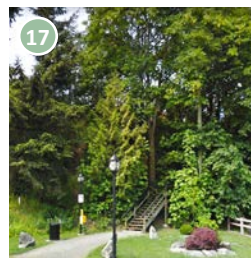
The open space around Cowichan Valley Open Learning Co-op has a playing field, playground, and court that are often used for public recreation after school hours.



St. Andrews Presbyterian Church operates a community garden which is used by church members and residents of the Cairnsmore neighbourhood.



Centennial Park, close to Cairnsmore, has a spray park, community gardens, cobb oven, playground, tennis and basketball courts, exercise equipment, covered picnic area, lawn bowling, and trails.



Heiwa Park, located at the bottom of the Canada Ave stairs, has gardens and green space and supports a pedestrian connection between Cairnsmore and Canada Avenue.



Somenos Marsh and the Cowichan Dyke Trail System are nearby providing nature and accessible walking trails; however access is complicated by the Trans Canada Highway.



PART B:

**POLICIES &
GUIDELINES**

INTERPRETATION OF LANGUAGE IN THE POLICIES

The following sections provide more detailed policies specific to Cairnsmore, intended to support the implementation of the vision and key directions.

Where a descriptive section accompanies a policy, it is provided for information purposes only to enhance the understanding of the policy.

Where "shall" is used in a policy, the policy is mandatory. However, it should be recognized that where quantities or numerical standards are given, such quantities or standards may be varied at the discretion of the approving authority, so long as the intent of the policy is still achieved and the variation is necessary to address unique circumstances that would otherwise render compliance impractical or impossible.

Where "should" is used in a policy, the intent is that the policy is encouraged, but can be varied to provide for actions that would generally satisfy the intent of the policy.

Where a policy requires submission of studies, analysis or other information, the City shall determine the exact requirements and timing of the submissions.



An example of sustainable land use with appropriately-scaled mixed use commercial and residential in Qualicum Beach's downtown.

4 | SUSTAINABLE LAND USE, BUILDINGS & SITES



A neighbourhood plan provides guidance on how land will be used in the future. This section provides an overview of policies on the land use locations, densities, built form and site design, green buildings, and heritage considerations appropriate in Cairnsmore. These policies will shape the built environment in support of the vision and key directions.

IN THIS SECTION:

- General policies for managing land use
- A land use plan and associated land use policies for the designations identified
- Guidance on built form and site design
- Green building requirements
- Heritage considerations

4.1 OVERVIEW

Together with the OCP, the Cairnsmore Sustainable Neighbourhood Plan provides a framework for future land uses. Within this framework, policies provide for a range of possible uses and leave room for individual decisions to be made related to the needs of owners, the interests of the community, and the unique context of a site.

The OCP and neighbourhood plan do not take the place of zoning. It is the City's Zoning Bylaw which confers legal development permissions for any property.



WHAT DO KEY DOCUMENTS SAY ABOUT SUSTAINABLE LAND USE?

COWICHAN 2050:

The CVRD region is anticipated to grow by at least 30% over the next 30 years. Cowichan 2050 encourages compact growth over sprawl to protect rural, agricultural, and environmentally significant lands.

INTEGRATED COMMUNITY SUSTAINABILITY PLAN:

The ICSP indicates a desired state where Duncan has a diverse, green, and flexible housing stock that meets all income and age requirements and is compact and mixed use, providing walkability to neighbourhood services. It envisions Duncan retaining its small-town character and its heritage, while supporting population growth and diversity.

OFFICIAL COMMUNITY PLAN:

OCP Section 5.1: Growth Management encourages and strongly supports smart growth principles.

OCP Section 5.2: Housing supports provision of diverse housing options through infill, new development, and redevelopment, while ensuring new development is a “good fit” with a neighbourhood’s character and ensuring that Duncan’s limited land base is used effectively by supporting higher densities in appropriate locations.

OCP Section 6.1: Economic Development encourages mixed use developments in close proximity to commercial areas as a means of increasing the residential population to encourage economic growth.

OCP Section 6.3: Other Commercial Areas supports mixing ground-level commercial and upper-level residential in neighbourhood nodes, including Cairnsmore.

COMMUNITY ENERGY AND EMISSIONS PLAN:

Action 5.2: Land Use Suite “Enhanced” encourages densified growth to help reduce emissions, recognizing that many residential preferences still tend towards single-family dwellings and recommend:

- Small lot / small home subdivisions within walking distance of town centre
- Fee simple row housing
- Allowing carriage houses / tiny homes
- Allowing suites in more residential zones
- Design, orientation, and landscaping for privacy

AGE-FRIENDLY PLAN:

The plan includes recommendations for increasing higher density housing where it is possible to walk to services and increase affordable housing through suites, co-ops, or other options.



WHAT IS SMART GROWTH & WHY IS IT IMPORTANT?

Smart Growth is a collection of land use and development principles that aim to enhance quality of life, preserve the natural environment, and save money over time. It encourages neighbourhoods that are diverse, livable, and walkable. Smart Growth is especially important in communities that have limited developable lands such as Duncan. Many of Cairnsmore's guiding principles align with Smart Growth.

SMART GROWTH PRINCIPLES:

- 1 Mix land uses:** Each neighbourhood has a mixture of homes, retail, businesses, and recreational opportunities.
- 2 Build well-designed compact neighbourhoods:** Residents can choose to live, work, shop, and play in close proximity. People can easily access daily activities, transit is viable, and local businesses are supported.
- 3 Provide a variety of transportation choices:** Neighbourhoods are attractive and have safe infrastructure for walking, cycling, transit, and driving.
- 4 Create diverse housing opportunities:** People in different family types, life stages, and income levels can afford a home in the neighbourhood of their choice.
- 5 Encourage growth in existing communities:** Investments in infrastructure, such as roads and schools, are used efficiently, and developments do not take up new land.
- 6 Preserve open spaces, natural beauty, and environmentally sensitive areas:** Development respects natural landscape features and has higher aesthetic, environmental, and financial value.
- 7 Protect and enhance agricultural lands:** A secure and productive land base that provides food security, employment, and habitat is maintained by focusing new development to urban areas.
- 8 Utilize smarter and cheaper infrastructure and green buildings:** Green buildings and other systems can save both money and the environment in the long run.
- 9 Foster a unique neighbourhood identity:** Each community is unique, vibrant, diverse, and inclusive.
- 10 Nurture engaged citizens:** Places belong to those who live, work, and play there. Engaged citizens participate in community life and decision-making.

4.2 GENERAL LAND USE POLICIES

ADMINISTRATION

- 1 The land use designations and policies in this section shall inform all development application decisions in the neighbourhood area. The boundaries, uses, and densities shown may be varied to account for unique circumstances, adaptability over time, and flexibility of implementation, so long as the intent of the policy is achieved and is consistent with the vision and key directions of this plan.
- 2 The City shall update the OCP Land Uses and Zoning Bylaw zones consistent with this plan. The policies and guidelines in this plan shall function as development permit guidelines.
- 3 The City should consider adding an Established Neighbourhood Overlay on established parts of Cairnsmore (and Duncan) where character protection is desired and provide associated design guidelines through establishment of an Intensive Residential Development Permit Area, Heritage Conservation Area, or other means to provide a mechanism for reviewing new infill development that could affect character of the neighbourhood.

NEIGHBOURHOOD CONTEXT

- 4 The City shall work with Island Health, CVRD, and Municipality of North Cowichan to plan in advance for how relocation of the Cowichan District Hospital and future redevelopment of the current hospital site will affect the Cairnsmore neighbourhood.

NEIGHBOURHOOD BOUNDARY

- 5 The City shall work with Municipality of North Cowichan to review municipal boundaries to the north and west of Cairnsmore to create a logical division between communities and incorporate bisected lots and contiguous land uses.
- 6 If boundary changes occur, the City shall amend this land use plan to incorporate added areas.

- 7 Several prominent sites, as indicated on Map 7 on page 39, mark entry points or significant viewpoints within Cairnsmore. Future development or redevelopment at these sites shall include provision of a visible focal point that celebrates entry into the neighbourhood.

NEW DEVELOPMENT

- 8 The City shall ensure that all new development, renovation, or rehabilitation in Cairnsmore is sensitive to the scale and form of neighbouring properties.
- 9 The City shall support and facilitate development of a range of housing types, sizes, and tenures in the neighbourhood. This should include housing options for seniors, lower-income individuals, families, and those with special needs.
- 10 The City should require development applicants to obtain public input on all new development applications and should encourage applicants to engage with the neighbourhood early in their process.
- 11 The City shall set a maximum impervious area for all types of development to preserve and enhance green space and support rainwater infiltration.
 - » Impervious areas include building roofs (excluding green roofs) and hard surface site areas (including asphalt, concrete, swimming pools).
 - » Pervious materials include landscape areas, green roofs, pervious paving, or other materials that allow rainwater to soak into the landscape.

Maximum impervious areas for land uses are listed in Section 4.4.

INFRASTRUCTURE

- 12 Existing infrastructure, including roads, sanitary sewer, and water, may not be adequate to service some developments anticipated by the neighbourhood plan. It is the developer's responsibility to evaluate required utilities and provide necessary upgrades resulting from an application to develop.

4.3 COMMUNITY AMENITY CONTRIBUTIONS

- 1 At the time of development application, the City shall negotiate with applicants for the provision of suitable Community Amenity Contributions that are proportionate to the development rights and value created through rezoning.
- 2 Community Amenity Contributions shall be in the form of agreed upon physical amenities, benefits, and/or cash-in-lieu contributions.
- 3 Where the provision of public access and open space would negatively affect the density provisions of a private development site, the City should consider land swap, density bonus, or other incentives.
- 4 The following community amenity contributions should be prioritized for Cairnsmore:
 - » Land for new / expanded public rights-of-way identified in this plan
 - » Parkland dedication and development above and beyond the statutory requirements (5%) dedication, provided it is suitable for public park use
 - » Semi-public open spaces including plazas and squares within the neighbourhood commercial node, but only if there is a legal agreement established on title to preserve public access
 - » Public amenities including children's play space, public art, and street furniture
 - » Affordable housing with suitable agreements to ensure units remain affordable for a set period of time
 - » Streetscape improvements

WHAT ARE COMMUNITY AMENITY CONTRIBUTIONS?

Growth and development increase the population living in a neighbourhood, often leading to a demand for community amenities beyond what are provided by development cost charges.

During rezoning, local governments may negotiate with applicants to provide community amenity contributions that are proportional to the development being created and the impacts of this development on the community.

Community amenity contribution facilitate the provision of amenities such as public parks, trails, and plazas; benefits, such as affordable housing; or cash-in-lieu that serve existing and new residents while mitigating impacts from densification. Community amenity contributions should be applied within the area affected by new development.



Photo: Sergio Ruiz

ABOUT LAND USE IN CAIRNSMORE

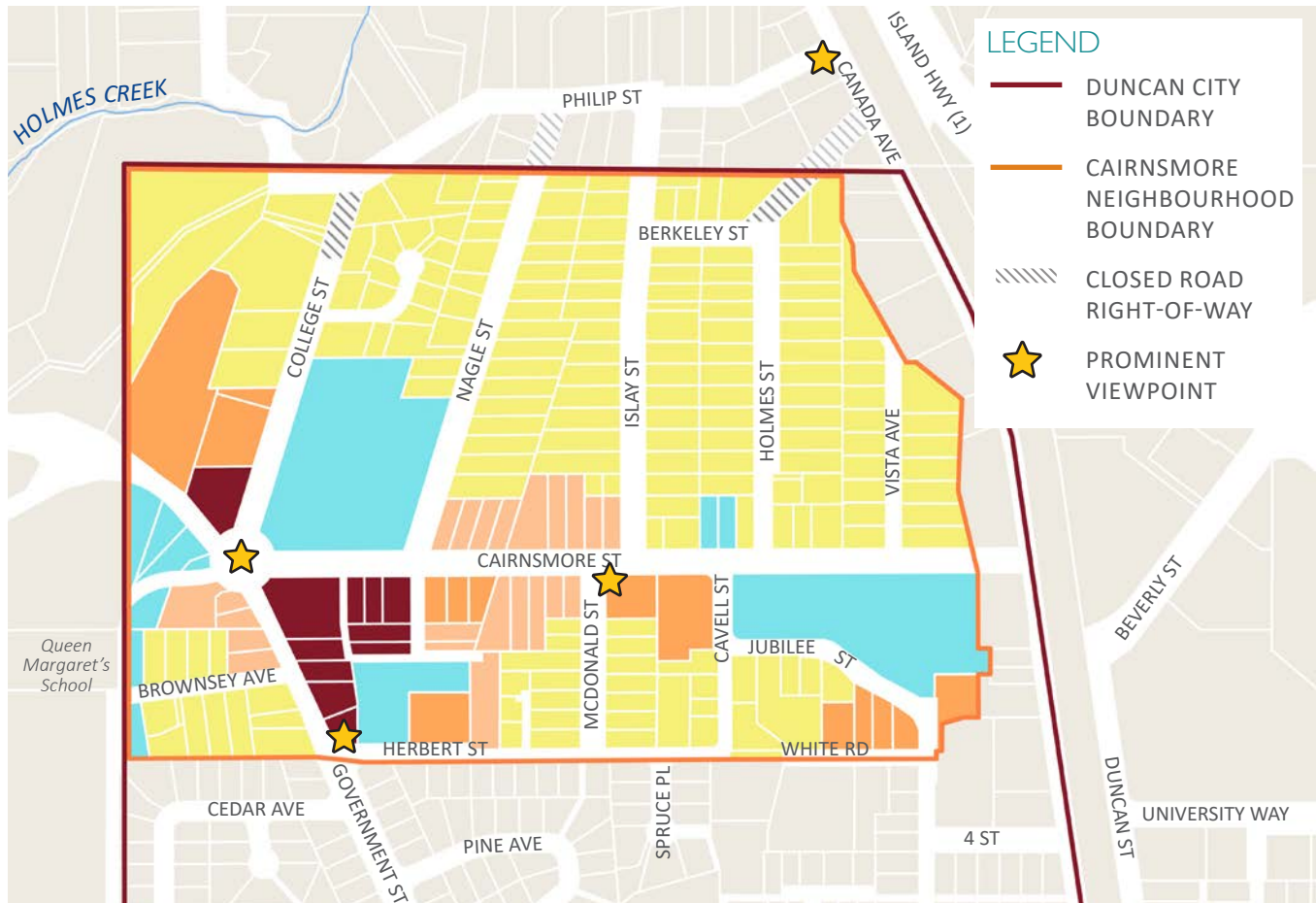
The Cairnsmore Sustainable Neighbourhood Plan land use plan focuses on maintaining and enhancing the unique character of the area. The plan centres medium density land uses and commercial activity around Cairnsmore's neighbourhood commercial node, creating a mixed neighbourhood where people can access much of what they need within walking distance of their homes. Other parts of the community remain low density for the life time of this plan, allowing limited smaller-scale growth through the incorporation of secondary suites. High density land uses are not envisioned for Cairnsmore at this time.

This plan recognizes a desire by the community to effectively transition between low and medium density land uses. To this end a transitional low / medium density land use designation has been created as a sensitive bridge between these areas.

Institutional uses will remain a large part of Cairnsmore's future, with some transition of institutional uses to active commercial and residential uses around the neighbourhood commercial node to support a strategy of activating the space and creating more places for people to live.



4.4 LAND USES



Map 7: Cairnsmore Neighbourhood Land Use Map

LAND USE	SEC.	PURPOSE IN CAIRNSMORE	BUILDING TYPES
NEIGHBOURHOOD COMMERCIAL MIXED USE	4.3.1	Provide local commercial services that support an active neighbourhood commercial node within a mixed use layout that provides high-quality residential above the first floor	<ul style="list-style-type: none"> » Mixed Use Commercial » Residential above ground floor
MEDIUM DENSITY RESIDENTIAL	4.3.2	Maintain and add high-quality medium density close to commercial areas and transit	<ul style="list-style-type: none"> » Townhouses » Houseplexes » Garden apartments » Live / Work Units
TRANSITIONAL LOW / MEDIUM DENSITY RESIDENTIAL	4.3.3	Transition between medium and low density residential areas to minimize impacts to existing residences and create a cohesive neighbourhood feeling	<ul style="list-style-type: none"> » Townhouses » Houseplexes » Duplexes » Live / Work Units
LOW DENSITY RESIDENTIAL	4.3.4	Continue existing residential form and character, provide homes on larger lots	<ul style="list-style-type: none"> » Single Family Home » Duplex » Secondary Suites
INSTITUTIONAL / COMMUNITY	4.3.5	Provide services to the local community and surrounding area	<ul style="list-style-type: none"> » Form varies, depending on use

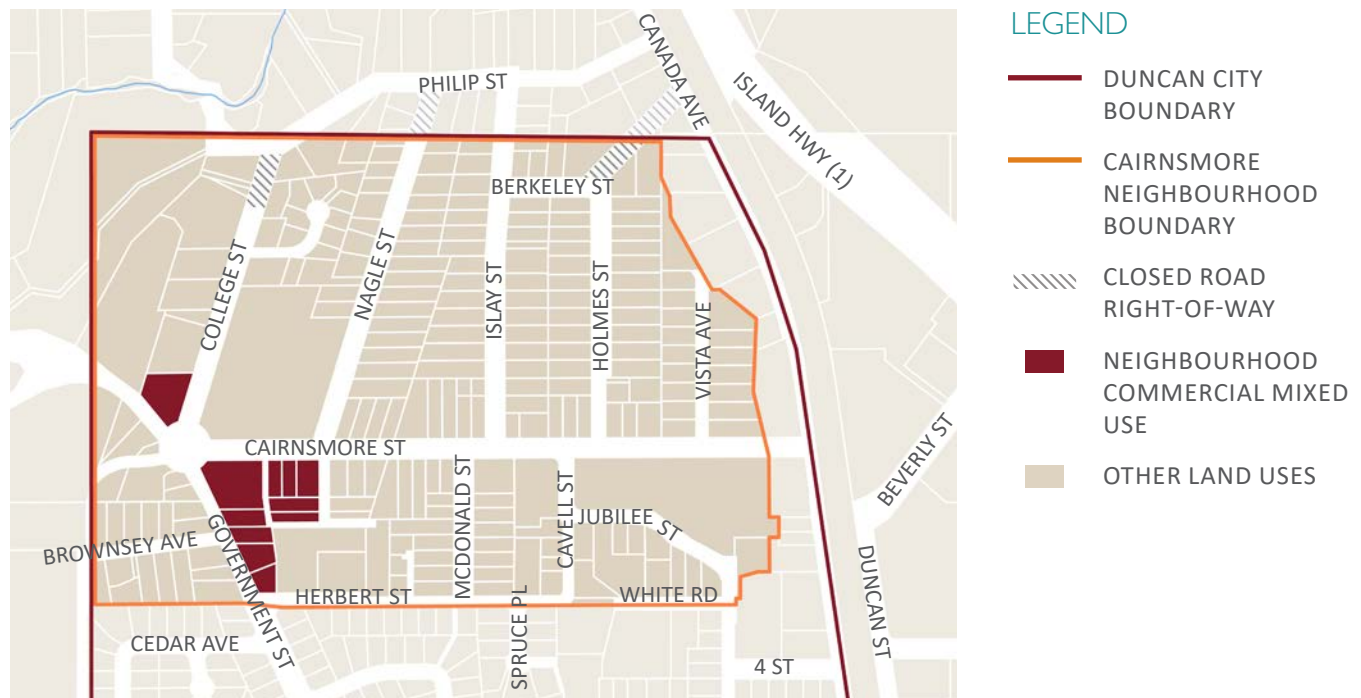
4.4.1 NEIGHBOURHOOD COMMERCIAL MIXED USE



INTENT

Neighbourhood commercial mixed uses will be focused within the neighbourhood commercial node, creating a focal point for neighbourhood-scale services, amenities, and jobs, with multi-family residential uses integrated above commercial uses.

LOCATIONS



Map 8: Neighbourhood Commercial Mixed Use Locations

PREFERRED USES

- Local-scale retail and services
- Multi-family residential (above first floor)
- Community care facility (above first floor)
- Office (above first floor)
- Artisan industry
- Public open space
- Community services
- Educational services

BUILDING TYPES

- Mixed Use Commercial
- Residential above ground floor

SETBACKS & FRONTAGES

- Minimum front setback = 0 m
- Maximum front setback = 3.0 m
- Active commercial uses facing the street
- Frontages to create a consistent street wall

BUILDING HEIGHTS & DENSITIES

- Minimum height = 2 storeys
- Maximum height = 4 storeys
- Minimum Floor Area Ratio = 1.0:1
- Maximum Floor Area Ratio = 2.0:1
- Buildings 3 storeys must have 70% or more underbuilding parking
- Buildings 4 storeys must have 90% or more underbuilding parking
- Maximum impervious area = 80%

KEY GUIDELINES

- 1 The ground floor of mixed use buildings shall have active commercial uses that generate foot traffic and pedestrian interest such as retail stores, food and drink establishments, personal services, entertainment, medical services.
- 2 Residential uses are permitted above the first floor only.
- 3 Residential parking shall be located in structures or underground as indicated in accordance with the heights and densities shown. Short-term off-street parking for customer uses, as required, shall be provided behind the building and may be surface or under building parking. No off-street surface parking shall be permitted between the front facade of the building and the street.
- 4 Retail commercial development should facilitate long-term adaptability to a variety of commercial uses.
- 5 In residential apartments, variation in unit sizes, shall be provided including:
 - » 2 and/or 3 bedroom units that can accommodate families
 - » Studio units suitable for individuals or couples with lower incomes including seniors

EXAMPLE TYPOLOGIES



EXAMPLE: Four-storey mixed use building with ground-floor commercial uses oriented to the street and residential uses above.



EXAMPLE: Three-storey mixed use building with varied facades and roof lines to provide architectural character.



EXAMPLE: Active commercial at the ground floor with a strong connection between indoor and outdoor spaces.

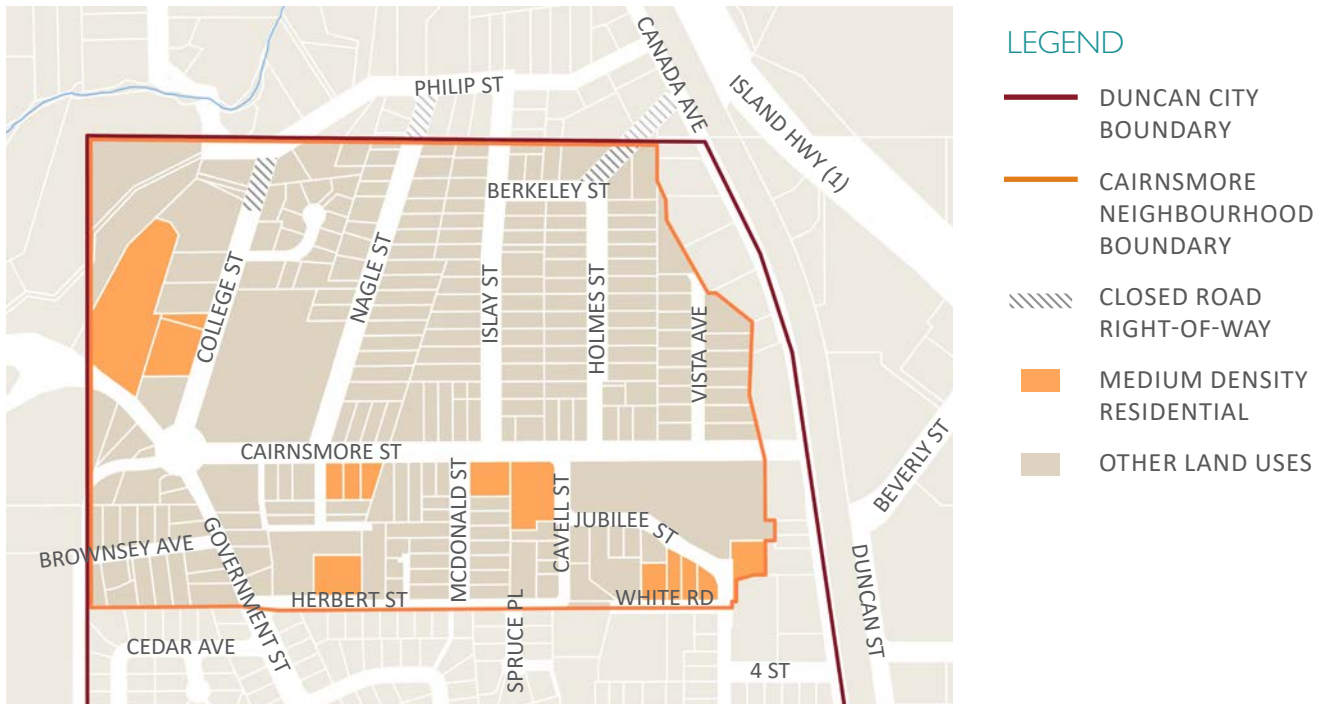
4.4.2 MEDIUM DENSITY RESIDENTIAL



INTENT

Medium density residential provides a range of apartment, townhouse, and houseplex styles located within walking distance of commercial areas and providing a range of housing options with high quality design and open spaces. Sensitively incorporating medium density residential options supports regional and city goals for accommodating growth, increases housing options, and helps maintain low density in other established areas.

LOCATIONS



Map 9: Medium Density Residential Locations

PREFERRED USES

- Residential
- Live-work ground-oriented units where compatible with the surrounding area
- Public open space

BUILDING TYPES

- Townhouses
- Houseplexes
- Garden apartments
- Live / Work Units

SETBACKS & FRONTAGES

- Minimum front setback = 3.0 m
- Maximum front setback = 5.0 m
- Frontages to address streets and public open spaces

BUILDING HEIGHTS & DENSITIES

- Minimum height = 2 storeys
- Maximum height = 4 storeys
- Minimum Floor Area Ratio = 0.8:1
- Maximum Floor Area Ratio = 1.5:1
- Buildings 4 storeys or higher than a FAR of 1.2: 1 must provide all parking underground
- Maximum impervious area = 65%

KEY GUIDELINES

- 1 Semi-private front yards that provide an attractive street front while providing a sense of separation and privacy for residents.
- 2 Off-street parking shall be provided behind or below buildings. Off-street parking between the front facade of the building and the street will not be permitted.
- 3 In garden apartments, variation in unit sizes, shall be provided including:
 - » 2 and/or 3 bedroom units that can accommodate families
 - » Studio units suitable for individuals or couples with lower incomes including seniors
- 4 The City shall support live / work units in the medium density residential designation that allow residents to live and work in the same unit, subject to the following conditions:
 - » Only low impact work uses shall be permitted (e.g., local retail, services, or office), including, but limited to: professional offices, hair salons, fitness studios, art galleries, etc. Uses that generate high foot traffic such as food services will not be permitted
 - » The appearance of live / work units is expected to be ground-oriented, semi-commercial units facing the street. The work portion of the unit shall be located on the ground floor and have direct access from the street
 - » Short-term patron parking for live / work commercial uses is to be provided on the street; parking for live / work residential uses is to be off-street, located behind the building.
 - » The maximum space allocated to working space in a live / work unit is 40%
 - » A live / work unit shall not result in a disturbance to the peace and quiet of the neighbourhood, nor may it cause the emission of dust, noise, odor, smoke, electronic interference, bright lights, or other nuisance

EXAMPLE TYPOLOGIES



EXAMPLE: Four-storey garden apartment with surrounding trees and landscaping.



EXAMPLE: Three-storey townhouses with an attractive facade, front landscaping, and a connection to the street.



EXAMPLE: Existing large home converted to a multi-unit houseplex which is indistinguishable from single-family dwellings.

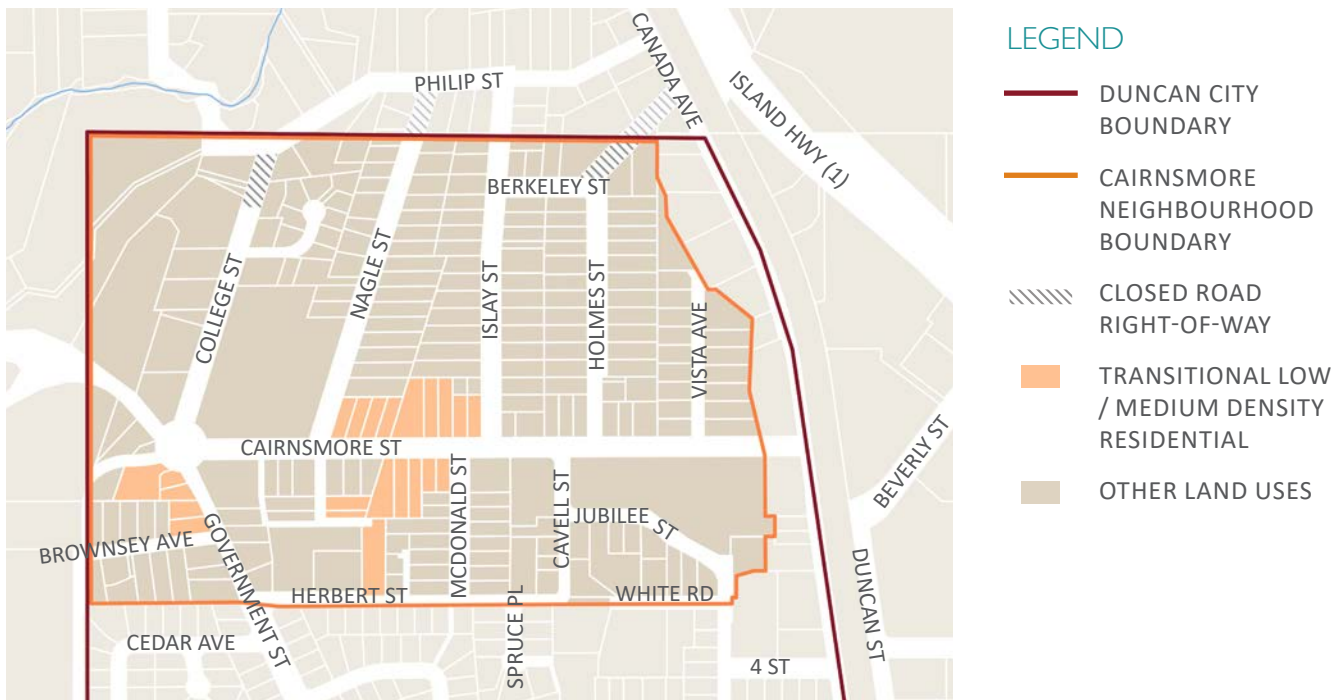
4.4.3 TRANSITIONAL LOW / MEDIUM DENSITY RESIDENTIAL



INTENT

The transitional low / medium density residential land use provides a flexible transition between higher intensity uses including mixed use and medium density residential around the neighbourhood commercial node and low density residential areas to create a thoughtful transition.

LOCATIONS



Map 10: Transitional Low / Medium Density Residential Locations

PREFERRED USES

- Residential
- Live-work ground-oriented units where compatible with the surrounding area
- Public open space

BUILDING TYPES

- Townhouses
- Houseplexes
- Duplexes
- Live / Work Units

SETBACKS & FRONTAGES

- Minimum front setback = 3.0 m
- Maximum front setback = 5.0 m
- Frontages to address streets and public open spaces

BUILDING HEIGHTS & DENSITIES

- Minimum height = 1 storey
- Maximum height = 2.5 storeys
- Minimum Floor Area Ratio = 0.25:1
- Maximum Floor Area Ratio = 1.1:0
- Maximum impervious area = 65%

KEY GUIDELINES

- 1 The City shall strongly encourage conversion of existing large homes into houseplex apartments with multiple units within the transitional low / medium density residential area.
- 2 The City should consider providing additional density or appropriate variances where an owner chooses to adapt an existing large home.
- 3 Form and massing for ground-oriented units in transitional low / medium density residential should accentuate individual units with clearly defined front entries.
- 4 Semi-private front yards oriented to the street that provide an attractive front while providing a sense of separation and privacy for residents shall be provided.
- 5 The City shall support live-work units in the transitional low / medium density residential designation that allow residents to live and work in the same unit, subject to the following conditions:
 - » Only low impact work uses shall be permitted (e.g., local retail, services, or office), including, but limited to: professional offices, hair salons, fitness studios, art galleries, etc. Uses that generate high foot traffic such as food services will not be permitted
 - » The appearance of live / work units is expected to be ground-oriented, semi-commercial units facing the street. The work portion of the unit shall be located on the ground floor and have direct access from the street
 - » Short-term patron parking for live / work commercial uses is to be provided on the street; parking for live / work residential uses is to be off-street, located behind the building.
 - » The maximum space allocated to working space in a live / work unit is 40%
 - » A live / work unit shall not result in a disturbance to the peace and quiet of the neighbourhood, nor may it cause the emission of dust, noise, odor, smoke, electronic interference, bright lights, or other nuisance

EXAMPLE TYPOLOGIES



EXAMPLE: A new 2.5-storey townhouse with clear front entries and a varied facade.



EXAMPLE: A large heritage home in Victoria transformed into a houseplex while maintaining the original form and character.



EXAMPLE: Two-storey duplex with side driveway access to parking.

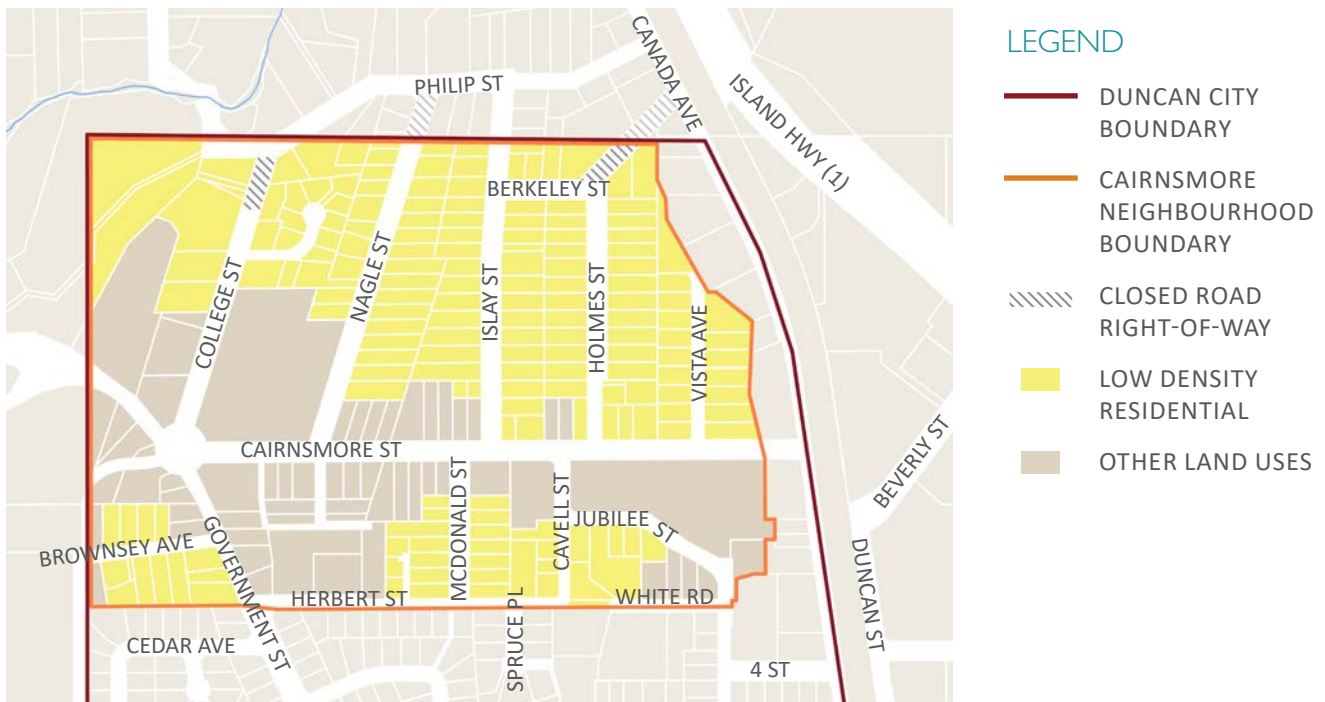
4.4.4 Low Density Residential



INTENT

Low density residential areas are an important component of Cairnsmore's character and as such will continue to be the dominant land use form for the lifetime of this plan – about 60% of developable land in the neighbourhood. This land use will include a mix of single family, duplex, and secondary suites that fit the existing Cairnsmore character.

LOCATIONS



Map 11: Low Density Residential Locations

PREFERRED USES

- Residential
- Home-based business where compatible with the surrounding area
- Secondary suites
- Public open space

BUILDING TYPES

- Single Family Home
- Duplex
- Secondary Suites

SETBACKS & FRONTAGES

- Front yard setbacks for new infill development to fall between the front yard setbacks of the two adjoining properties. If there are not homes on both adjoining properties or there is a discrepancy of more than 1.5 m in the setbacks, align with the average setback of the other houses on the street

BUILDING HEIGHTS & DENSITIES

- Minimum height = 1 storey
- Maximum height = 2.5 storeys
- Minimum floor area ratio (FAR) = none
- Maximum floor area ratio (FAR) = 0.75:1
- Maximum impervious area: 50%

KEY GUIDELINES

- 1 The City should strongly discourage subdivision requests that require variances to lot frontage or lot depths that contradict the principles of maintaining green space, heritage, and trees.
- 2 The City should discourage panhandle lots.
- 3 Accessory dwellings shall be encouraged in low density residential areas that have single-unit dwellings as a means of increasing housing options and supporting affordability.
- 4 New accessory dwellings should be designed to use materials and features that are similar to the main residence.
- 5 Off-street parking for accessory dwellings shall be required at a rate of 1 stall per dwelling. The City should consider relaxation of this requirement if:
 - » Creation of new off-street parking could not be achieved without removal of existing mature trees
 - » The City deems there to be sufficient on-street parking available
- 6 Homes shall be sized appropriately to retain trees on lots. Massive homes that dominate the lot, landscape, and street are not the intention of this designation.
- 7 Where subdivision of larger lots is proposed for low density residential, the City shall:
 - » Consider density bonus provisions that encourage clustering of new development to preserve existing mature trees or wooded areas and to provide both larger and smaller parcels that support the objective of variation in lot sizes, housing forms, and character
 - » Encourage consideration for alternative housing formats including housing co-ops

EXAMPLE TYPOLOGIES



EXAMPLE: A new single-family residential home integrated with the scale and character of the existing neighbourhood, but with a modern twist.



EXAMPLE: A two-storey duplex with separate entries.



EXAMPLE: Clustered homes providing space for preservation of mature trees and shared open space.

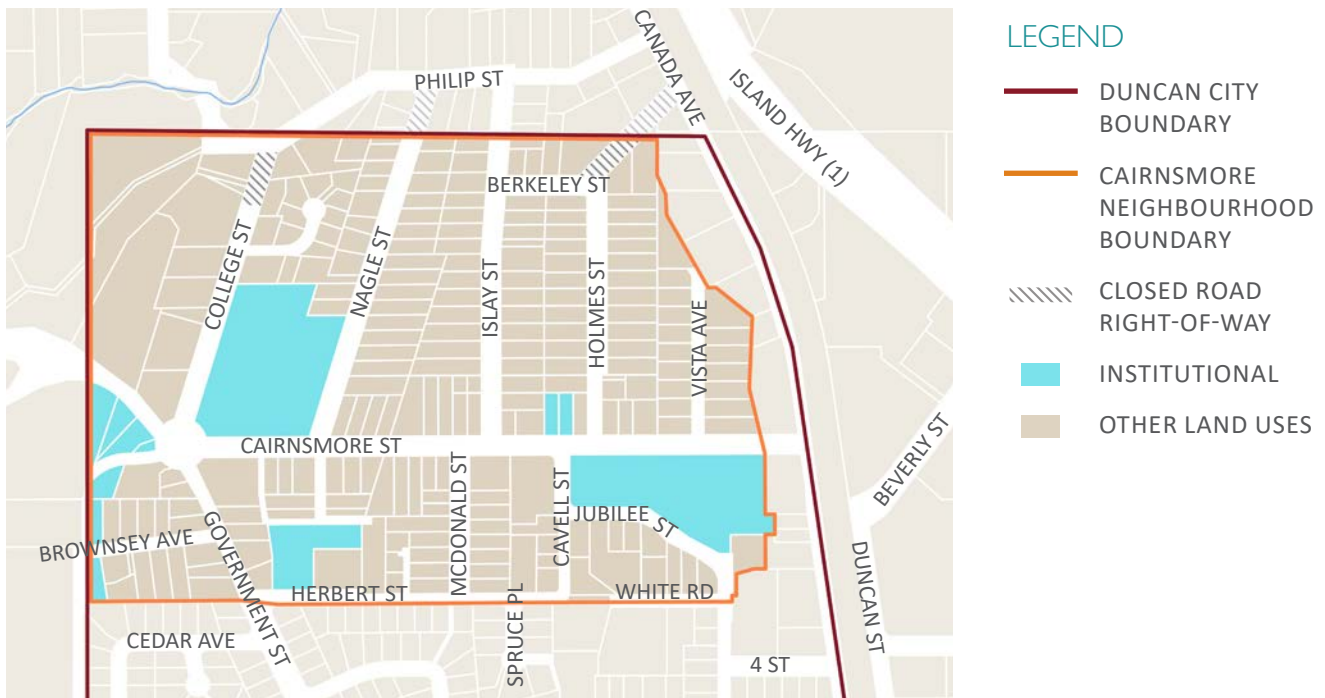
4.4.5 INSTITUTIONAL



INTENT

Institutional uses are a key part of Cairnsmore, bringing employment and activity to the area. These spaces have an important role to play in creating a neighbourhood for people. Opportunities to link these spaces to the public realm will help facilitate connections and a feeling of community.

LOCATIONS



Map 12: Institutional Locations

PREFERRED USES

- Health care
- Education
- Community services
- Recreational facilities
- Public open space

BUILDING TYPES

- Form varies, depending on use
- To be determined on a project-by-project basis by City staff, informed by the OCP and neighbourhood plan

SETBACKS & FRONTAGES

- Varies depending on use
- Prioritize relationship between front facade of the building and the street and limit parking between the building and street

BUILDING HEIGHTS & DENSITIES

- Varies, depending on use
- Height to consider impacts on adjacent uses

KEY POLICIES

- 1 Redevelopment or expansion of educational or civic sites shall incorporate publicly accessible open spaces.
- 2 Building design of health care facilities should incorporate semi-private spaces that connect to the public realm and encourage social interaction.



GROWING GRACEFULLY

Concerns about increased density are common in established neighbourhoods, including how changes could impact community character, property values, and community safety. However, it is important to recognize that our population is growing, and growth, when handled gracefully comes with positive gains including economic development, community amenity, and opportunities to live in a neighbourhood through all life stages and circumstances.

A GOOD IDEA

The Fairfield Neighbourhood in Victoria is exploring the concept of "Gentle Density" that proposes an approach to growth that focuses on sensitive infill or conversions of existing character homes to accommodate multiple families, often side-by-side with single family homes.



Cairnsmore's future neighbourhood commercial node will support local-scale businesses and be a meeting space for residents.

4.5 NEIGHBOURHOOD COMMERCIAL NODE



The neighbourhood commercial node already plays an important role in the small-scale, walkable feel of Cairnsmore. The future neighbourhood commercial node will build on this role by renewing and adding local commercial opportunities, incorporating new multi-family residential to support an active commercial area, increasing walkability and pedestrian comfort by connecting building entrances to the street, and strategically adding public open space.

OVERVIEW

Today the neighbourhood commercial node in Cairnsmore includes a few businesses oriented around the corner of Cairnsmore and Government Streets. This plan envisions this node evolving into a more active, pedestrian-oriented neighbourhood commercial node while retaining the locally-oriented character of the area.

A unique advantage of Cairnsmore's neighbourhood commercial node is institutional uses within the area and nearby. This links the commercial area with schools, daycares, and care facilities, drawing prospective customers and residents to the area.



WHY MIXED USE?

When done well, mixed use developments offer neighbourhoods many benefits, including:

- Bringing people closer to what they need on a day-to-day basis, reducing reliance on automobiles and offering independence to people who are unable to drive
- Increasing walkability and reducing automobile use, helping to reduce traffic and congestion
- Minimizing the need to extend and maintain additional services
- Providing opportunities for residents unable to access low density housing including young people, single-parent families, workers, and the downsizing elderly, building in potential to live in a neighbourhood throughout a lifetime
- Increasing positive activity in the area with people coming and going during different times of day and evening, decreasing isolation and the associated negative behaviours that can happen when there are not "eyes on the street"
- Focusing the growth needed to accommodate our rising population to a smaller footprint, helping to preserve existing low density areas
- Supporting establishment and retention of prosperous businesses by providing a customer base in the immediate vicinity
- Facilitating the creation of valued public assets including parks, plazas, and improved streetscapes helping create opportunities to meet and interact with neighbours

Mixed use living is not for everyone. But it does expand the range of living options available in a neighbourhood, allowing residents to find choices that work for their lifestyles, incomes, and life stages.









NEIGHBOURHOOD COMMERCIAL NODE CONCEPT

The following concept sketch provides a potential future for the neighbourhood commercial node, but is illustrative only. Actual future development will be based on property owner proposals with input from the community.



Figure 10: Neighbourhood Commercial Node Concept

LEGEND

	NEIGHBOURHOOD COMMERCIAL MIXED USE		INSTITUTIONAL
	MEDIUM DENSITY RESIDENTIAL		CYCLING NETWORK
	TRANSITIONAL LOW / MEDIUM DENSITY RESIDENTIAL		PEDESTRIAN NETWORK
	LOW DENSITY RESIDENTIAL		CAIRNSMORE NEIGHBOURHOOD BOUNDARY

- ① Enhanced existing medium density residential
- ② Future medium density residential oriented towards College Street with a strong facade and streetscape improvements
- ③ Future transition of vehicle-oriented commercial to local-scale commercial mixed use
- ④ Improved College Street streetscape with street trees and sidewalks
- ⑤ New roundabout with central feature marking the entrance to the Cairnsmore neighbourhood
- ⑥ New road from roundabout that provides entry / exit to Queen Margaret's School and access to residential properties
- ⑦ Widened road right-of-way along Government Street to incorporate Complete Street enhancements including street trees, bike lanes, and wide sidewalks
- ⑧ Mixed use local commercial with residential above oriented to Government and Cairnsmore Streets
- ⑨ Potential covered plaza space with seating and meeting space associated with new mixed use development
- ⑩ Access to underground residential parking with limited surface parking for commercial uses
- ⑪ Feature celebrating entry into Cairnsmore
- ⑫ Closure of existing lane exiting to Herbert Street
- ⑬ With SD #79 and CVOLC, collaborative improvements along Cairnsmore Street frontage to better utilize underused areas and enhance community recreation, consider potential for future mixed or community uses, and connect the school to the neighbourhood commercial node
- ⑭ Improved streetscape along Cairnsmore Street with street trees, bike lanes, on-street parking, and wide sidewalks
- ⑮ Potential location for new park space central within the neighbourhood commercial node with uses like children's play and open space
- ⑯ New lane network developed to provide access to parking behind and beneath buildings and designed to provide a pleasant mixed environment that supports shared vehicle, cyclist, and pedestrian access
- ⑰ Transitional low / medium density residential including houseplexes or townhouses up to 2.5 stories high to provide a stepped transition from mixed uses to adjacent low density residential and to provide potential for live / work opportunities



Shared lane in mixed use area



Street-oriented local commercial



Lighting for evening safety



Public gathering space

NEIGHBOURHOOD COMMERCIAL NODE POLICIES:

APPROPRIATE USES

- 1 New commercial uses in Cairnsmore shall be local businesses compatible with the neighbourhood-scale of the area. New auto-oriented uses including car dealerships, automobile service centres, gas stations, parking facilities, big box businesses, and drive-throughs are not supported. Businesses with significant noise and/or traffic impacts such as funeral service facilities and night clubs are not supported.
- 2 New development in the neighbourhood commercial node shall include one larger commercial retail space in the range of 600 m² to 800 m² to support local grocery services.

ROAD NETWORK & STREETSCAPES

- 3 A new laneway system shall be developed in the neighbourhood commercial node – refer to Figure 10 for general location and arrangement. Lanes should have streetscape treatments such as pavers, a raised street table, trees, chicanes, or other traffic calming features that emphasize shared use.
- 4 Government Street road right-of-way shall be widened to 20 m through the development application process. Opportunities for land swap of unused portions of the existing City-owned lane should be explored (see Section 5.4).
- 5 Main streets within the neighbourhood commercial node including Government Street and Cairnsmore Street shall be Complete Streets that accommodate all modes of transportation and prioritize pedestrian activity including wide sidewalks, seating areas, street furniture, bicycle parking, pedestrian lighting, landscaping, wayfinding, and other features (see Section 5.5).
- 6 Driveways and parking access shall be located to minimize interruption of commercial frontages and pedestrian walkways.
- 7 Boulevards in front of retail frontages shall use attractive hardscape surfaces that support a large pedestrian area. Ample root zones are required around street trees in hardscape (see Section 6.2).



EXAMPLE. A commercial node creates an animated streetscape by encouraging businesses to spill into the sidewalk and creating an attractive pedestrian realm.



EXAMPLE. Complete streets integrate all modes of transportation including cycling, walking, and vehicles.



Photo: Eric Sehr

EXAMPLE. A shared lane provides comfortable spaces for all modes of travel and has attractive elements such as trees, decorative paving, bollards, and lighting that slow vehicle speeds and create a welcoming space for all.

PARKING

- 8 All off-street parking shall be beneath or behind buildings with access from the lane network. No off-street parking shall be permitted between a building facade and the street.

PUBLIC SPACES

- 9 A public plaza space shall be created on Cairnsmore Street. The plaza should be oriented to have connection with multiple businesses and provide attractive design that encourages social interaction. Weather protection over some or all of the plaza should be considered.
- 10 A public park in the neighbourhood commercial node shall be created to provide space for children's play, recreation, and community gathering. Integrated stormwater management may be included within the park area if there is sufficient benefit without loss of adequate publicly usable recreation space.
- 11 The City should collaborate with SD#79 and Cowichan Valley Open Learning Co-op to enhance the Cairnsmore Street school frontage and create an inviting urban landscape that supports the neighbourhood commercial node. Ideas could include an updated playground area, a welcoming entry to the school, improved basketball courts, or other elements that make it feel like a welcoming community destination, while respecting non-school uses may be limited during learning hours.
- 12 Incorporate landmarks to mark entry into Cairnsmore at the following "gateway" locations:
 - » Corner of Herbert Street & Government Street
 - » Corner of Cairnsmore Street & Government Street (roundabout)

Features may include public art, signage, attractive paving materials, ornamental lighting, or other visible landmarks.



EXAMPLE. A covered plaza creates comfortable places to sit or meet through all seasons.



EXAMPLE. A basketball court is designed to incorporate seating and surrounding green space that invites use and enjoyment.



EXAMPLE. Welcome signs identify the neighbourhood and support neighbourhood identity.



The architecture of a building, how it addresses surrounding public spaces, and the site it sits on are all part of creating a successful neighbourhood.

4.6 BUILT FORM & SITE DESIGN



The character of a neighbourhood is influenced by design and quality of the site and buildings within it. With a building's life span typically well over 50 years, it is important that any new development or redevelopment that occurs supports progress towards the desired vision. Poor development kills momentum; great development builds success, attracting more high quality investment.

OVERVIEW

Cairnsmore is an established neighbourhood with a strong existing character; therefore, a sensitive approach to new development is warranted.

The purpose of the following policies is to guide the scale and form of development in support of the vision and key directions of this plan. New buildings are intended to contribute to a coherent, unified, and engaging neighbourhood environment.

The function, character, architectural style, and configuration of buildings, as well as their relationship to streets and open spaces, are key elements of built form and site design.

4.6.1 BUILDING ARCHITECTURE

- 1 All new buildings shall have a strong and cohesive architectural style that:
 - » Introduces variety between buildings
 - » Creates visual interest and pedestrian-scale details at the street and sidewalk level
 - » Utilizes high quality and authentic exterior building materials that draw from materials common in the neighbourhood such as wood, stucco, and metal. Materials such as brick, faux stone, and vinyl, should be avoided or used sparingly
 - » Provides varied and articulated facades that limit large expanses of plain walls
 - » Provides a defined and welcoming entrance oriented to the street using building elements such as porches, raised entries, windows, and roof extensions
 - » Avoids the appearance of buildings turning their backs or sides toward the street
- 2 A modern interpretation of heritage building styles that exist in the neighbourhood today (e.g., Cottage, Craftsman, Rancher) is appropriate and encouraged. Strongly themed architectural styles that are not characterized in the neighbourhood's architectural history should be avoided.
- 3 Buildings in the neighbourhood commercial node should incorporate the following building features:
 - » Alignment of commercial facades to create a consistent street edge and "wall"
 - » Glazing on at least 50% of the street-facing, ground-floor facade
 - » Inclusion of residential balconies on upper storey units facing the street or public open spaces to support passive surveillance
 - » Awnings and/or overhangs that provide weather protection over all entrances and along primary retail streets
 - » A fine-grained commercial frontage development pattern at the ground floor, regardless of the floor area of the units
- 4 Introduction of thematic architectural styles associated with chain businesses is not supported.
- 5 On corner lots, both building elevations facing the street shall be given equal treatment and must be compatible with other buildings on the respective adjoining streets.
- 6 The City should consider allowing aerial encroachments into the public right-of-way for appropriate building overhangs or awnings that provide desirable weather protection.



NOT THIS. New residential (left) does not have form and character consistent with its neighbour and does not address the street.



THIS. Townhouses with articulated facades and main entrances, windows, porches, and landscaping oriented to the street.



EXAMPLE. Canopies in a commercial area provide weather protection for pedestrians in the retail area.

4.6.2 BUILDING SITING & MASSING

- 1 New buildings shall be of a mass and siting that respects existing buildings by:
 - » Minimizing shadowing on residential floors as well as public or private open spaces
 - » Ensuring sensitivity to the privacy of existing residences by placing or recessing balconies to avoid direct views into neighbouring private spaces
 - » Limiting light trespass
 - » Avoiding imposing view restrictions
- 2 All third and fourth storeys of buildings shall be setback a minimum of 2.0 m to reduce the mass of the building and to limit shading impacts on streets and adjacent buildings.
- 3 New development or redevelopment shall consider impacts to existing mature trees:
 - » Development proponents shall document and assess existing mature trees prior to siting structures and pavement
 - » Development proponents shall design the site to protect existing mature trees wherever possible.
 - » Development proponents shall submit a tree removal and protection plan within the development submission that clearly shows trees to remain and protection measures to be used as well as trees to be removed, including rationale and mitigations including new tree planting (see Section 6.2)
 - » The City should consider appropriate variances that support retention of mature trees
- 4 Buildings shall be sited within required front yard setbacks outlined in Section 4.4:
 - » Closer front yard setbacks should be favoured to create a vibrant street edge and provide sufficient private backyard space
 - » Where no setback is required, any setback provided shall only be to enhance the public realm by providing high quality open space
- 5 Development proposals shall complete a solar shading analysis that illustrates solar access for all units within a building and impacts on adjacent properties.



NOT THIS. A new development is significantly out of scale to the existing adjacent home and has potential shading impacts.



THIS. A new fourplex provides a similar scale and character to adjacent residential.



EXAMPLE. Buildings that step back on the third and fourth storeys help reduce the mass of the building and limit shading impacts. Setbacks also provide outdoor living spaces for upper residential units.

4.6.3 SITE & LANDSCAPE

- 1 Landscape architecture plans, prepared by a registered landscape architect, shall be provided in all development applications at the developer's expense.
- 2 Building siting shall take into account the existing landscape and all possible efforts should be made to retain existing mature trees. Where existing mature trees have to be removed they shall be replaced as outlined in Section 6.2.
- 3 Building footprints for all mixed use and multi-family development should be located to create opportunities for usable outdoor spaces including plazas, courtyards, and patios.
- 4 Storage areas shall be located to the rear of buildings or screened with appropriate landscaping.
- 5 Site planning shall fully consider active transportation connections and shall provide access and linkages accordingly. Proposals that impede existing or planned active transportation connections as outlined in Section 5.2 shall not be supported.
- 6 New tree planting will be required for all surface parking areas associated with mixed use, medium density, transitional low / medium density, and institutional proposals and shall include tree plantings designed to result in 50% shading of parking lot surfaces within 15 years subject to the following:
 - » The amount of shade provided by a given tree is determined by using the mature tree crown's square footage
 - » Overlapping shade does not count twice
 - » Street trees and existing trees may be included in shade calculations
 - » Trees must be provided with adequate soil volumes

A tree shading plan and associated calculations will be prepared by a licensed professional. The landscape plan may be suitable provided trees are drawn to scale at 15-year maturity.
- 7 Stormwater management measures including rain gardens, infiltration swales, infiltration trenches, and pervious paving shall be strongly encouraged.
- 8 Rooftop gardens should be encouraged.
- 9 All new development or redevelopment shall provide landscape areas that include the following conditions:
 - » Front yard landscapes shall provide an attractive pedestrian environment to adjacent streets
 - » Fully paved or graveled front yards shall not be permitted. Front landscaping shall be provided and should be complementary to adjacent landscapes
 - » Low water use landscapes should be encouraged
 - » Accessible travel routes shall be provided from adjacent roadways and parking areas to main building entrances and shall conform to the requirements of the most current edition of the BC Building Code.



NOT THIS. A fully paved and graveled front yard does not provide a welcoming interface with the street.



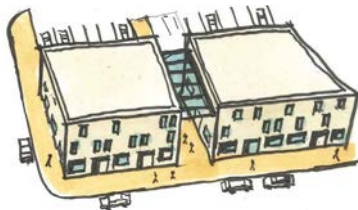
THIS. A landscaped front entry connects with the street.

4.6.4 ACCESS & PARKING

- 1 In all mixed use and medium density residential areas:
 - » Underground parking shall be required in order to achieve the heights / densities as outlined in Section 4.4
 - » Where off-street surface parking is provided, it shall be located to the rear of buildings and shall be accessed by lanes wherever possible. Off-street parking located to the side may be considered if rear access is not possible
 - » Off-street parking between the front face of the building and the public sidewalk shall be prohibited
 - » Surface parking areas shall be screened by landscaping
 - » Surface parking areas shall include clearly defined pedestrian circulation routes
 - » Large surface parking areas should use decorative paving, textures, and vegetation to break up large expanses and shall have tree cover as outlined in Section 4.6.3
 - » Service areas should be located behind buildings and screened with fencing and/or landscaping



NOT THIS. Parking between the street and building.



THIS. Parking behind the building with mid-block access.



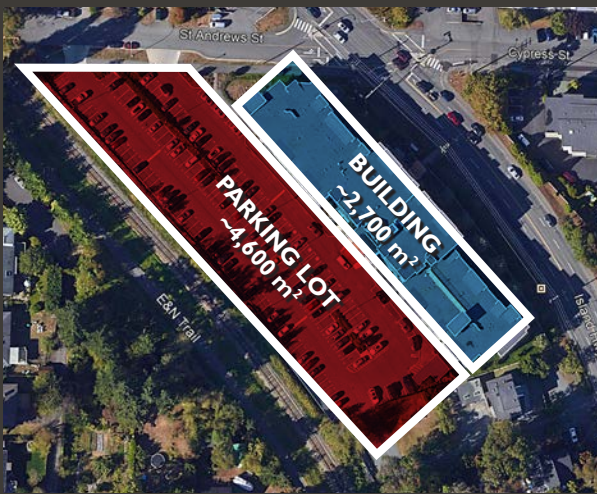
Photo: La Citta Vita

EXAMPLE. Trees and textured paving breaks up large expanses of parking areas and creates an attractive environment.

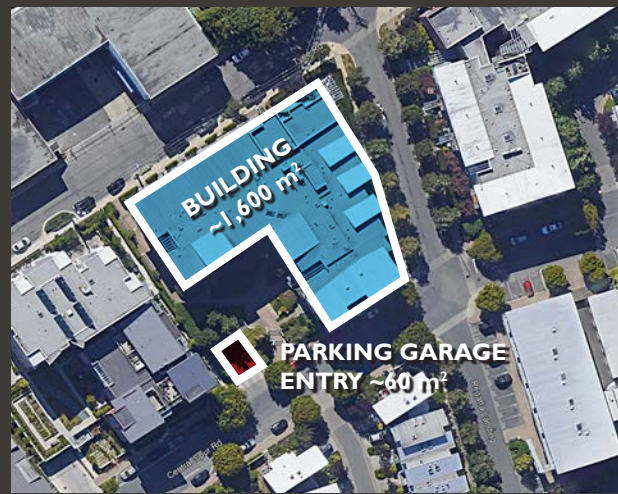
- 2 In low density residential areas, parking and parking structures shall be located to maintain a connection between homes and the street and provide space for front yard landscaping:
 - » Garages and carports shall not be located in front of homes or protrude beyond the front facade of the home
 - » Garages and carports shall be located behind homes (preferred) or set back from the front facade
 - » Driveways and uncovered parking shall be located to the side of the front facade of the home (not in front)
- 3 All new multi-family, mixed use, and institutional development or redevelopment shall be required to provide bicycle parking. Weather-protected and secure facilities should be considered.
- 4 Shared parking and accesses shall be encouraged to reduce the number of curb let-downs that interrupt pedestrian and cycling movements.
- 5 The City should consider allowing reduction in required off-street parking stalls based on the type of units planned and alternate transportation measures made available including provision of car share vehicles, subsidized transit passes, and long-term bicycle parking.
- 6 New development shall incorporate provisions for electric vehicle charging as follows:
 - » At least five publicly-accessible electric vehicle charging stations shall be provided in the neighbourhood commercial node through development. The City should increase this number at its discretion as demand rises
 - » All new ground-oriented residential buildings shall include the required power connections to support installation of at least one charging station for electric vehicles at the time of development or to support retrofitting at a later time
 - » In all new or redeveloped apartment residential buildings, a dedicated number of electric charging stations shall be provided. The City will work with the proponent to establish this number based on current and anticipated future electric vehicle demands at the time of application

ILLUSTRATING PARKING TRADE-OFFS: SURFACE PARKING VS. UNDERGROUND PARKING IN MIXED USE AND MEDIUM DENSITY RESIDENTIAL AREAS

Depending on the type of development being considered, one or more parking stalls are typically required for each unit. The illustrative examples below demonstrate the difference in area required to accommodate surface parking versus underground parking for a four-storey apartment building. Note: this is an illustrative example only and the areas shown are approximate based on air photo interpretation.



Site Size = 10,000 m²
Building Area = 2,700 m² (27% of site)
Surface Parking Area = 4,600 m² (46% of site)



Site Size = 2,700 m²
Building Area = 1,600 m² (60% of site)
Surface Parking Area* = 60 m² (2% of site)
* Entry to underground parking

Stacked underground parking is desirable in cities where space is limited. This reduces the surface area dedicated to parking, providing more space for building units, pedestrian connectivity, public or private green space, stormwater management, or alternative uses.



Green buildings and sites are needed to support Duncan's commitment to becoming more resilient to climate change.

4.7 GREEN BUILDINGS & SITES



The City of Duncan is committed to addressing climate change, reducing greenhouse gas (GHG) emissions, reducing energy and water consumption, and preparing for a more resilient future. Developing green buildings and sites is an important component of a rounded approach to reducing our community's environmental footprint.

OVERVIEW

The City's Integrated Community Sustainability Plan envisions Duncan transitioning toward the elimination of GHG emissions by 2040, a progression towards efficient use, conservation, and diversity of renewable energy, and new development with energy conservation and efficient building design.

In 2015 the City developed a Community Energy and Emissions Plan to identify actions towards reducing greenhouse gas emissions in the community.

Smart Growth that mixes land uses, will create complete neighbourhoods and will reduce reliance on vehicles, supporting Duncan's progress to a more sustainable community (see Section 4.4).

Equally important will be developing smarter, greener buildings. This section outlines policies about green buildings and sites related to energy conservation, greenhouse gas emissions reduction, water conservation, and rainwater management.

4.7.1 ENERGY CONSERVATION & GHG EMISSIONS REDUCTION

- 1 All new mixed use and multi-family buildings in Cairnsmore shall be required to achieve Step 3 of the BC Energy Step Code.
- 2 The City should consider providing density bonus incentives for new buildings that achieve upper steps (Steps 4,5).
- 3 Building design should incorporate use of solar and other renewable energy systems.
- 4 Building design and orientation shall:
 - » Consider passive heating, lighting, and cooling features
 - » Optimize benefits of solar orientation
 - » Locate landscaping to incorporate natural daylight and seasonal shading needs
- 5 All new buildings should be designed and engineered to support future adaptation to sustainable technologies such as solar thermal and grey water reclamation.
- 6 The City should encourage owners of older housing stock to convert to efficient energy systems and address energy-loss through education and incentives, including promotion of programs available through energy providers and other levels of government.
- 7 The City should encourage the elimination of oil heating systems by educating home owners about rebate programs.
- 8 The City should consider excluding renewable energy components (such as solar collectors) from building height restrictions.

4.7.2 WATER CONSERVATION

- 9 The City shall provide education and information that encourages property owners to create low-water use landscapes through retention of native trees and shrubs and drought-tolerant landscapes.
- 10 Landscaped areas in all new mixed use and multi-family developments shall require high-efficiency irrigation systems:
 - » Irrigation plans prepared by a professional shall be submitted with the development application and will include information about equipment and watering schedule. Water conservation measures should be noted
 - » All landscaped areas shall be watered by an automatic irrigation system complete with a "smart" controller and rain sensor
- 11 The City should encourage rainfall capture systems in new and existing development for use in irrigation where possible.
- 12 Development projects should incorporate grey water for irrigation where feasible.

4.7.3 RAINWATER MANAGEMENT

- 13 All new development or redevelopment should minimize impervious areas and incorporate on-site integrated stormwater management that maintain pre-development infiltration rates and site hydrology.
- 14 A rainwater management plan shall be prepared by a professional engineer and submitted with all development applications and will calculate pre- and post-development flows on the site.
- 15 Wherever possible, water shall be drained from impervious surfaces to pervious rainwater capture areas.
- 16 The City should adopt maximum impervious areas for all new development (see Section 4.4).
- 17 The City should provide education on and encourage property owners to minimize impervious areas, maintain existing trees, incorporate rain gardens and infiltration swales, use pervious paving for patios and driveways, and use other methods to increase infiltration on private lots.



The existing Duncan Elementary building is part of Cairnsmore's built heritage.

4.8 BUILT HERITAGE



Heritage buildings tell stories of a place's history and provide a visual history for future generations. This section provides an overview of Cairnsmore's built heritage and policies for continuing to preserve and celebrate these assets.

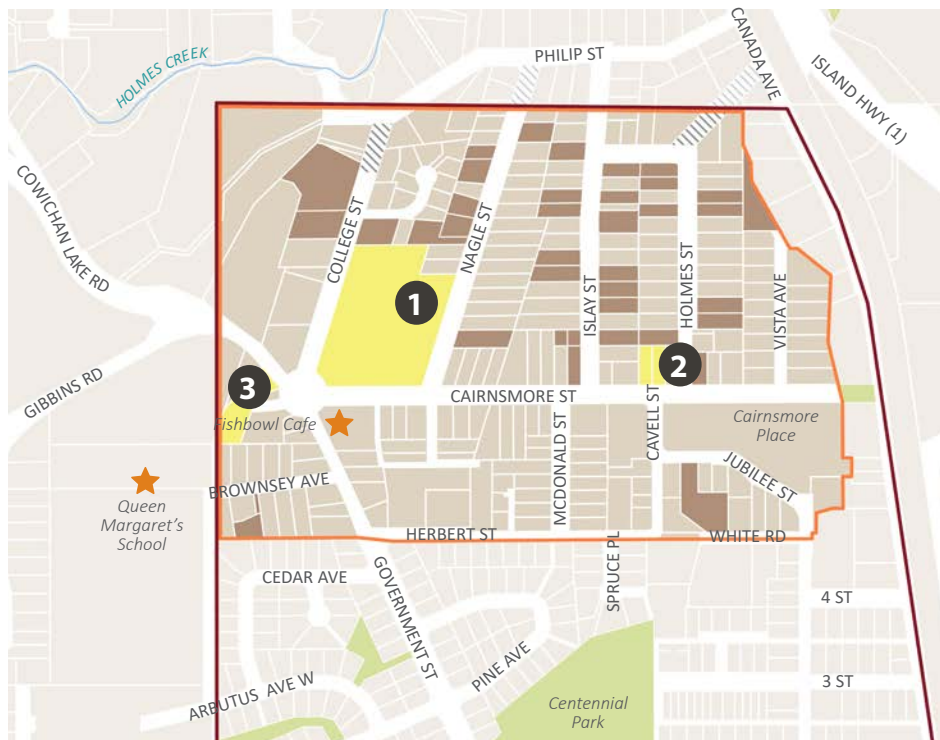
OVERVIEW

The Integrated Community Sustainability Plan notes that heritage preservation and restoration programs in Duncan should be a model of innovation, showcasing a fusion of heritage and contemporary design.

The Cairnsmore neighbourhood represents an era in Duncan's development as a city. In 1911, a new subdivision was laid out on land owned at the time by Reverend David Holmes. Over the next decade, homes were developed in the subdivision named "Buena Vista Heights" (also known locally as "Hospital Hill"). Several homes built in the Buena Vista Heights era remain today, showcasing building styles of the time.

Cairnsmore is home to three of Duncan's eight heritage-designated building (see Map 13). These designated properties are afforded heritage protection under the *BC Heritage Conservation Act*.

While Duncan does not have a formal heritage registry for other sites or buildings, past volunteer efforts have inventoried a number of potential sites of heritage merit. The City's informal *Heritage List* identifies 78 residential buildings with potential heritage merit in the City of Duncan; 34 of these in Cairnsmore.



LEGEND

- DUNCAN CITY BOUNDARY
- CAIRNSMORE NEIGHBOURHOOD BOUNDARY
- ▨ CLOSED ROAD RIGHT-OF-WAY
- PARK
- SITE WITH HERITAGE DESIGNATION
 - 1** Cowichan Valley Open Learning Co-op (former Duncan Elementary) - 1913
 - 2** Girl Guide Hall - 1925
 - 3** Queen Margaret's School Chapel - 1933
- SITE ON THE CITY'S HERITAGE LIST (informal)
- ★ OTHER SITE WITH POTENTIAL HERITAGE VALUE (not on current list)

Map 13: Existing Heritage Resources

HERITAGE-DESIGNATED BUILDINGS IN CAIRNSMORE



Duncan Elementary



Queen Margaret's School Chapel



Girl Guide Hall

SAMPLE OF RESIDENCES ON DUNCAN'S INFORMAL HERITAGE LIST



BUILT HERITAGE POLICES:

- 1 Support continued preservation of existing heritage-designated landmarks in Cairnsmore including Girl Guide Hall, Queen Margaret's Chapel, and Duncan Elementary.
- 2 The City should develop a City-wide heritage framework that:
 - » Identifies incentives and/or bylaws for managing heritage resources in the City
 - » Establishes a Heritage Register to more formally record sites of heritage value, affording improved protection of heritage resources
 - » Provides information about heritage conservation re-use and its benefits
 - » Identifies heritage awards or programs
- 3 When redevelopment is proposed for properties with heritage merit in Cairnsmore, the City should:
 - » Encourage retention and adaptive re-use of buildings and discourage demolition for new development
 - » Support sensitive building additions to allow conversion (e.g., single-family dwelling to duplex or houseplex)
 - » Support flexibility in site planning, building form, massing, parking, and other regulatory requirements that maintain heritage values
- 4 The City should support community partners or volunteers in documenting heritage values.

A GOOD EXAMPLE: NANAIMO'S HERITAGE CONSERVATION PROGRAM

Since 2002, the City of Nanaimo has been working to conserve and celebrate the historical buildings, structures, and sites that are part of Nanaimo's unique heritage. Since that time, the City has:

- Adopted a Community Heritage Register
- Created a Downtown Heritage Conservation Area that requires a Heritage Alteration Permit for non-cosmetic alterations made to building exteriors
- Created incentive programs to encourage conservation of recognized heritage buildings, and
- Produced numerous promotional materials designed to raise heritage awareness in the community.

Current programs include a Heritage Home Grant Program, Heritage Facade Grant Program, and a Heritage Tax Exemption Program.



A home on Nanaimo's Heritage Registry.



The future Cairnsmore is envisioned as a multi-modal neighbourhood that provides people choice in how they get around.

5 | MOBILITY



Cairnsmore's mobility network helps people connect both within and beyond the neighbourhood boundaries. Safe, attractive, and convenient connections that help residents, students, employees, and visitors get where they need to go in a way that works for them is a priority. This section provides details for upgrading Cairnsmore's active transportation, transit, and vehicle connections and provides guidance on creating attractive, Complete Streets for the future.

IN THIS SECTION:

- General policies for enhancing mobility
- Policies and improvements for active transportation
- Policies and improvements for transit
- Policies and improvements for the vehicle network
- Streetscape sections and policies to guide creation of Complete Streets

5.1 OVERVIEW

As population grows, both in Cairnsmore and surrounding areas, more people will need to get around safely and efficiently. To support this, the mobility network will need to enhance safety, comfort, and efficiency for all users.

The City of Duncan is developing a Transportation Master Plan that will provide detailed guidance and standards. It is intended that the Cairnsmore Sustainable Neighbourhood Plan be interpreted with reference to the Transportation Master Plan.

WHAT DO KEY DOCUMENTS SAY ABOUT MOBILITY?

COWICHAN 2050:

The plan recognizes that the CVRD region is currently auto-dependent and transportation is the leading generator of greenhouse gas emissions in the region. The plan anticipates that transportation services and technologies are likely to change considerably in the future and recommends a regional transportation framework, harmonized Complete Street standards, and engagement on emerging transportation technologies that will have local implications.

COWICHAN VALLEY REGION TRANSIT FUTURE PLAN:

The plan recommends improvements to the transit network in Duncan including creation of more direct neighbourhood routes.

INTEGRATED COMMUNITY SUSTAINABILITY PLAN:

The *ICSP Desired Outcomes for Transportation & Mobility* focus on shifting transportation uses from a reliance on vehicles to other modes of transportation and identifies cycling and walking becoming primary modes of local transportation in Duncan. This would be accomplished in part by a system of well-marked bike paths that connect neighbourhoods to schools, commercial areas, and regional trails.

OFFICIAL COMMUNITY PLAN:

OCP Section 6.3: Other Commercial Areas requires public and private sector initiatives to improve streetscapes and accessibility for pedestrians and cyclists in all commercial areas.

OCP Section 7.1: The Natural Environment encourages the integration of green space into the streetscape right-of-way to benefit the public realm.

OCP Section 7.2: Climate Protection sets targets to increase transit within the modal split for commuting trips and identifies a policy to facilitate efforts to increase transit route bus frequency throughout the community.

OCP Section 8.4: Transportation defines policies that:

- Encourage higher-quality pedestrian facilities, enhance walking routes, designate safe cycling routes, and help reduce residents' dependence on personal vehicles.
- Define a safe and efficient road network, minimizing major traffic flows into neighbourhoods, and continuing to accommodate a wide variety of uses.
- Recommend improvements to transit facilities and encourage safe, accessible pedestrian routes between transit stops and community destinations.

OCP Section 12: Development Permit Areas describes design guidelines for streetscapes.

COMMUNITY ENERGY AND EMISSIONS PLAN:

Action 6.4: Special Event Planning encourages people to use transit by making it free during special events.

Action 6.9: Low carbon and electric vehicle suite encourages electric vehicle charging stations.

DUNCAN AREA ACTIVE TRANSPORTATION PLAN:


The *Active Transportation Plan* identifies potential improvements and policies to “provide safe, convenient, and comfortable active transportation facilities to promote the movement of people and goods; maximize transportation choice; promote livability and sustainability; and minimize environmental impact.”

WHAT ARE COMPLETE STREETS?

Complete Streets are streets for everyone. They are designed to be safe and accessible to all users: people who walk, bicycle, take transit, or drive and people of all ages and abilities. Complete Streets make it easy to cross the street, walk to shops, and bicycle to work. Creating Complete Streets means designing and operating the entire road right-of-way to enable safe access, regardless of users' age, ability, or choice of transportation mode.

A GOOD EXAMPLE: SEATTLE, WA

In 2007, Seattle adopted a Complete Streets policy that was developed with a coalition of local advocacy groups. The policy recognizes that a Complete Street has a range of possible attributes and does not use a rigid template, but recognizes a need for careful design decisions based on data, land use, community experience, and anticipation of future needs.



A generous sidewalk provides space for pedestrians and amenities and creates an attractive streetscape including street trees, lighting, and urban design elements.

Vehicle lanes provide defined space for motorists.

A protected cycling facility separates cyclists from moving vehicles.

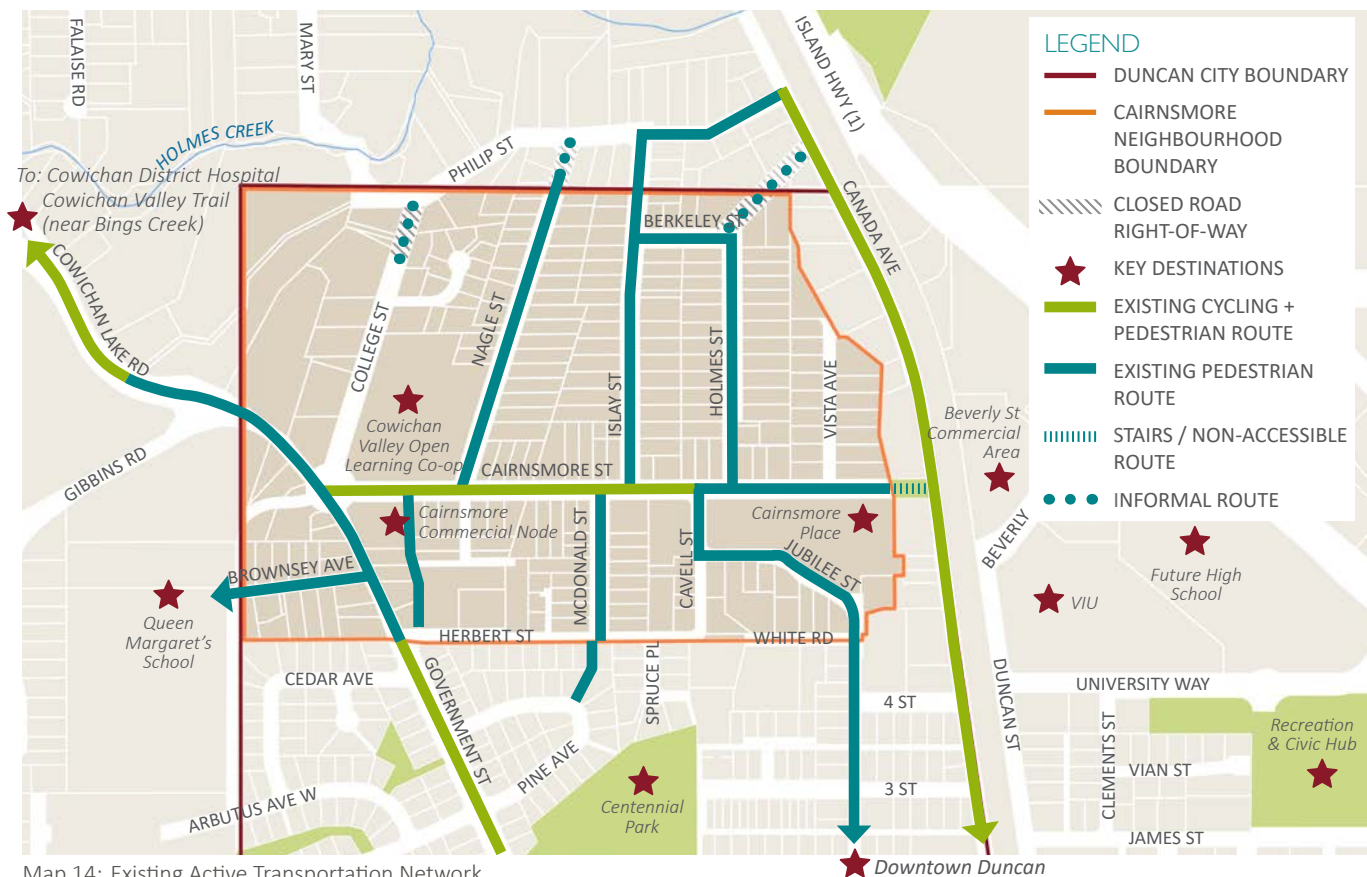
A floating bus stop provides a safe space for pedestrians to load onto the bus and hosts amenities like a bus shelter.

5.2 ACTIVE TRANSPORTATION

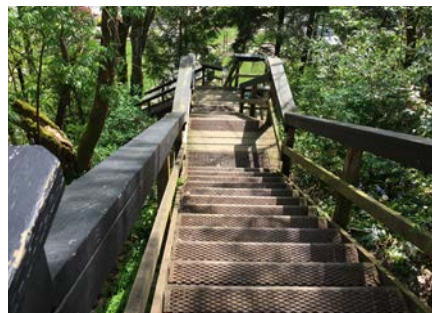
Active transportation is any form of human-powered transportation – walking, cycling, using a mobility device, in-line skating, skateboarding, and more. Cairnsmore's location – close to the downtown, the hospital, schools – makes active transportation achievable and desirable. Existing bike lanes and sidewalks are a starting point for a connected active transportation network.



Stats show that Cairnsmore has more residents who cycle to work (7.4%) than other parts of Duncan (2.4%).



Cairnsmore Street has a marked cycling route, but there are concerns about conflicts with curb bump-outs. Other primary transportation corridors, including Government and Jubilee Streets, do not have dedicated cycling routes.



The Canada Avenue stairs at the end of Cairnsmore Street connect Cairnsmore to nearby services, but there are concerns about negative behaviours in the area due to limited passive surveillance.



Most of Cairnsmore's residential streets have separated sidewalks on at least one side. However, some streets including College (shown), Herbert, Vista, and a portion of Cavell do not have any designated pedestrian facilities.



THE BENEFITS OF ACTIVE TRANSPORTATION



HEALTHY PEOPLE: Encourages physical activity by all ages and abilities helping to reduce health care costs, provides low-cost activities accessible to all income levels, supports both physical and mental health



LIVABLE COMMUNITIES: Helps attract and retain skilled labour and businesses, supports community renewal, increases safe routes to school, work, and play



SOCIAL WELL-BEING: Encourages interaction with neighbours, provides a source of community pride, encourages volunteerism, helps increase positive activity and passive surveillance and decrease anti-social behaviour



HEALTHY ENVIRONMENT: Provides transportation alternatives that reduce air pollution, encourages exercise and active living, has low environmental impacts

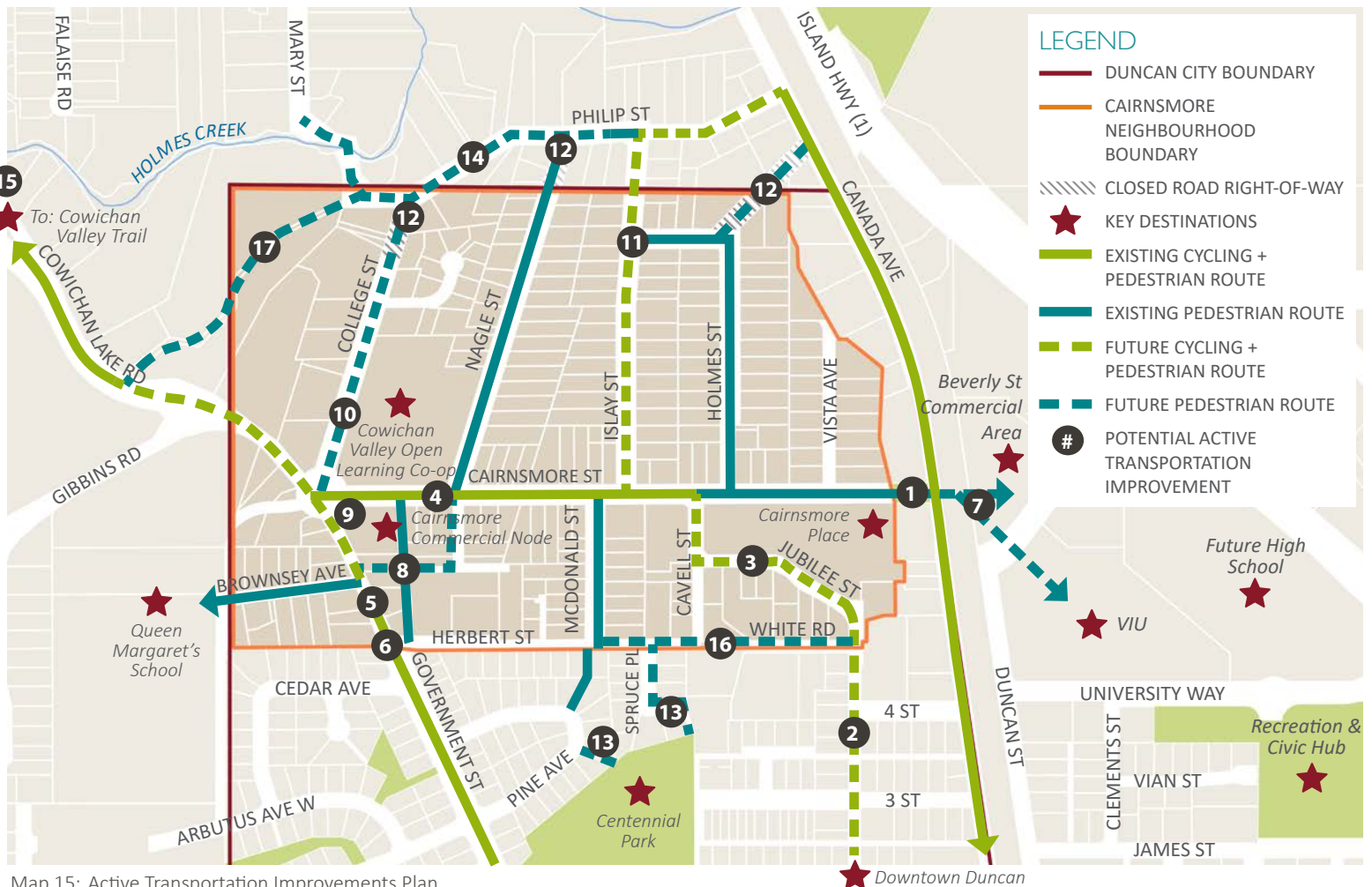


ECONOMIC BENEFITS: Supports local businesses, increases property values, supports tourism

ACTIVE TRANSPORTATION POLICIES

- 1 Active transportation improvements shall be the highest priority when any to transportation network improvements in Cairnsmore are undertaken.
- 2 The City shall pursue continuous pedestrian access along all neighbourhood transportation corridors.
- 3 Active transportation routes should:
 - » Provide accessibility for all ages and abilities
 - » Use separated cycling facilities wherever possible to reduce conflict between vehicles and cyclists
 - » Apply road design measures at intersections with sidewalks and bike lanes to maximize pedestrian and cyclist safety and raise awareness for all modes
 - » Provide adequate lighting (to current standards) at places where pedestrian and cycling routes cross roadways
- 4 When planning active transportation routes, the proponent shall include appropriate and complementary public realm improvements such as seating, transit stops, public art, lighting, street trees, wayfinding, bicycle parking, and other features.
- 5 When planning active transportation upgrades, Crime Prevention Through Environmental Design (CPTED) guidelines shall be followed

ACTIVE TRANSPORTATION IMPROVEMENTS PLAN



Map 15: Active Transportation Improvements Plan

ACTIVE TRANSPORTATION IMPROVEMENTS

HIGH PRIORITIES

- 1 The City should improve the staircase connection between Cairnsmore Street and Canada Avenue including lighting, sightline improvements, and tree limbing to increase passive surveillance. Improvements should consider fun or innovative features to activate the area such as informal play or public art.
- 2 On Jubilee Street from White Road to Downtown (outside neighbourhood), improvements identified in the Active Transportation Plan should be completed to better connect the Cairnsmore and Downtown neighbourhoods, including:
 - » Sidewalk widening and accessibility improvements that addresses conflicts from power poles on the sidewalks
 - » Addition of protected cycling lanes
- 3 Protected cycling facilities should be added on Jubilee Street from Cavell Street to White Road (multi-use shared pathway or protected cycling lanes and sidewalks) to improve safety on the hill.
- 4 Cairnsmore Street cycling facilities should be upgraded to remove curb bump-outs and add protected cycling facilities.
- 5 Active transportation facilities including protected cycling lanes should be provided on Government Street. Additions will require expansion of the road right-of-way (see Section 5.4).
- 6 The City should upgrade safety measures at the existing pedestrian crosswalk at the corner of Herbert and Government Streets such as flashing lights or a pedestrian-activated signals.
- 7 The City should improve pedestrian access across Canada Avenue and the rail line adjacent to the Canada Avenue Staircase to better connect Cairnsmore residents to the multi-use trail on the east side of Canada Avenue, businesses in the Beverly Street commercial area, VIU, and the future high school.
- 8 During development of the neighbourhood commercial node, a strong pedestrian network with shall be created.
- 9 The City should facilitate addition of a bike maintenance facility (e.g., air pump, tools) in the neighbourhood commercial node during future development.

MODERATE PRIORITIES

- 10 Pedestrian facilities on College Street should be added during future development and/or road upgrades.
- 11 Active transportation facilities should be added to Islay Street, including:
 - » A pedestrian crosswalk and visibility improvements at the corner of Islay and Berkeley Streets
 - » Protected cycling lanes from Cairnsmore to Philip Street and working with Municipality of North Cowichan to provide extension of an active transportation route to Canada Avenue
- 12 The City should formalize the following existing informal pedestrian connections to increase neighbourhood connectivity:
 - » College Street to Philip Street
 - » Nagle Street to Philip Street
 - » Holmes / Berkeley Street to Canada AvenueImprovements should incorporate CPTED principles such as improved sightlines and lighting. Improvements would need to be completed in partnership with the Municipality of North Cowichan where they cross jurisdictional boundaries.
- 13 Improve pedestrian routes and wayfinding from Cairnsmore to Centennial Park using existing rights-of-way. Stairs are likely required.
- 14 The City should encourage Municipality of North Cowichan to add sidewalks on Philip Street to increase pedestrian safety.
- 15 As part of regional planning, the City should promote opportunities to create an active transportation connection from the Cairnsmore neighbourhood to the Cowichan Valley Trail.
- 16 During future development, an active transportation connection between White Road and Herbert Street (staircase) should be prioritized.
- 17 During future development, an active transportation connection between Lake Cowichan Road and Philip Street (in collaboration with Municipality of North Cowichan), should be prioritized.

5.3 TRANSIT

In Cairnsmore, and throughout Cowichan Valley, transit is operated by BC Transit. There are currently two main transit routes that travel through or near Cairnsmore. The Lake Cowichan route formerly traveled on Government Street through Cairnsmore, but the route has since been changed to use Cliffs Road.



Stats show Cairnsmore residents do not use transit to commute to work compared to 2.7% of Duncan residents overall.

Route	Terminus	Key Connections	Frequency
2- Mt. Prevost	<ul style="list-style-type: none"> North: Cowichan Commons South: Village Green Mall 	<ul style="list-style-type: none"> Cowichan Lake Road to the hospital Cairnsmore St / Jubilee St in Cairnsmore 	<ul style="list-style-type: none"> 7 am to 9 pm Every 0.5 hrs during peak Every 1 to 1.5 hrs during non-peak Less frequent on weekends
3- Quamichan	<ul style="list-style-type: none"> North: Cowichan Commons South: Village Green Mall 	<ul style="list-style-type: none"> Lane Rd / Sherman Rd in MNC Canada Ave east of Cairnsmore 	<ul style="list-style-type: none"> 7 am to 9 pm Every 1 to 1.5 hrs on weekdays Every 2 hours on weekends

Figure 11: Transit Routes in / near Cairnsmore at Time of Neighbourhood Plan Development

EXISTING TRANSIT NETWORK MAP



Map 16: Existing Transit Network

TRANSIT POLICIES

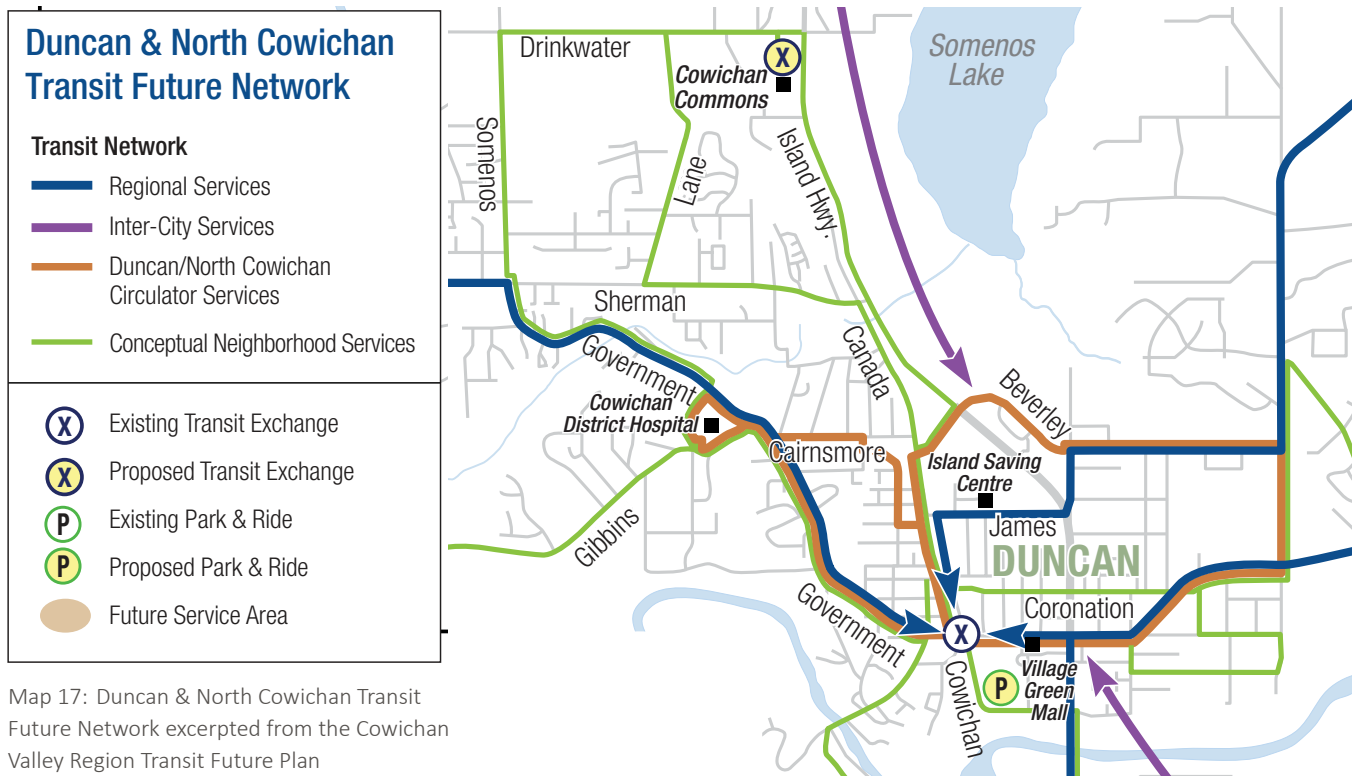
- 1 The City shall advocate for:
 - » Increased service frequency to Cairnsmore
 - » Routing improvements that provide circulator services and more direct trips between Cairnsmore and the Downtown Transit Exchange (as conceptually indicated in Map 17 below).
- 2 The City should create an attractive pedestrian realm at transit stops, including sidewalks, pedestrian crossings, lighting, shelter, and barrier-free design.
- 3 The City should seek opportunities to combine art and transit within the neighbourhood commercial node.



Well designed and maintained transit stops support beautification and a feeling of safety.



Transit stations can be art. "BottleStop" in Lexington, KY, designed by Aaron Scales, uses recycled glass soft drink bottles encased in safety glass and lit by solar panels.



MULTI-MODAL INTERSECTIONS

The majority of traffic incidents occur at intersections where multiple modes of transportation meet. Intersection improvements need to prioritize pedestrian and cyclist movements in order to create safe, well-defined routes and ensure road users expect other modes.



5.4 VEHICLE MOBILITY

Residents of Cairnsmore, as well as residents in surrounding areas, rely on the road network that runs through Cairnsmore. Growth in Cairnsmore and Municipality of North Cowichan neighbourhoods means more people will need to get around the area.

The existing road network is designed with local streets that connect into several major roads including Government Street, Cairnsmore Street, and Jubilee Street.

Key vehicle mobility concerns in Cairnsmore include:

- Congestion at the Cairnsmore / Government Street intersection
- Traffic volumes and speeds on Cairnsmore Street, Government Street, and Jubilee Street
- Use of local residential streets, such as Herbert Street, to cut through the neighbourhood
- Safety at the Cavell / Cairnsmore Street intersection which is a two-way stop
- How traffic affects institutional uses in or near Cairnsmore (schools, daycares, health centres) and how these uses increase traffic at key times (e.g., start / end of school day)



Stats show most Cairnsmore residents rely on vehicles to commute to work with 70.4% driving and 7.4% riding as a passenger.



The intersection at Government Street / Cairnsmore Street sees heavy use and is considered the most problematic traffic location in Cairnsmore. Plans are underway to convert the intersection to a roundabout.

ROAD SAFETY

Map 18 shows ICBC vehicle crashes and cyclist-involved collisions from 2013 through 2017.

Data indicates Cairnsmore is relatively safe for walking and cycling, with no pedestrian-related crashes and one cycling-related crash recorded within the neighbourhood boundary.

Vehicle crashes have occurred at main intersections, with the most at the Government / Cairnsmore Street intersection.



Map 18: ICBC Crash Data 2013-2017

VEHICLE MOBILITY OVERVIEW MAP



VEHICLE MOBILITY IMPROVEMENTS

- 1 The City shall build a roundabout at the Cairnsmore / Government / College intersection:
 - » The roundabout design shall include a new local road that provides access to Queen Margaret's School and residential lots
 - » The roundabout design shall have defined pedestrian and cyclist routes that provide the active transportation connections conceptually shown in Map 15 on page 72
 - » The roundabout design and surrounding streetscape should include placemaking features and urban design to mark a visible and attractive entrance to the Cairnsmore neighbourhood
- 2 A new lane network shall be developed in the neighbourhood commercial node as conceptually shown in Figure 10 on page 52:
 - » Lanes will be developed in City-owned rights-of-way where existing
 - » New lanes not in City-owned rights-of-way shall be dedicated as public road right-of-way through the development application process in accordance with the policies of this plan
- 3 The City shall secure, through land swap, development application, or purchase, expanded public road right-of-ways as follows:
 - » Government Street = 20 m min.
 - » Jubilee Street = 15 m min.
- 4 The City should convert the Cavell / Cairnsmore intersection to a three-way stop to manage traffic speeds and improve pedestrian safety.
- 5 The City should consider reducing the speed limit to 30 km/hr maximum on:
 - » Government Street from Herbert St to the traffic circle at Gibbins Road / Lake Cowichan Road
 - » Cairnsmore Street
- 6 The City should consult with local residents about traffic calming measures on Herbert Street to reduce cut-through traffic.
- 7 The City should work with Municipality of North Cowichan to analyze the suitability of opening the road right-of-way from College Street to Philip Street to improve network connectivity.



EXAMPLE. A decorative roundabout provides a point of visual interest while maintaining efficient traffic movement.

VEHICLE MOBILITY POLICIES

- 1 The City should work with CVRD and Municipality of North Cowichan to analyze traffic impacts to the Cairnsmore neighbourhood from all new development in adjacent North Cowichan neighbourhoods that will increase traffic in Cairnsmore and plan for how these impacts will be reduced and/or mitigated.
- 2 Future streetscape upgrades should be consistent with the Streetscape Sections shown in Section 5.5 and with the City's Transportation Master Plan. Where a discrepancy occurs, the Transportation Master Plan will take precedence.
- 3 Work with Queen Margaret's School to continue planning how future school growth will affect vehicle mobility in Cairnsmore and mitigations that may be required.

5.5 STREET TYPOLOGIES & STREETSAPES

Streets not only provide spaces for safe movement of people and goods; they play a fundamental role in the character and experience of a neighbourhood. Everyone living in the neighbourhood interacts with the street – whether driving, cycling, walking, or viewing from their homes. Great streets are memorable and define the experience and character of an area.

The following typologies and streetscape policies seek to enhance streets as public spaces that support the experience of the neighbourhood.

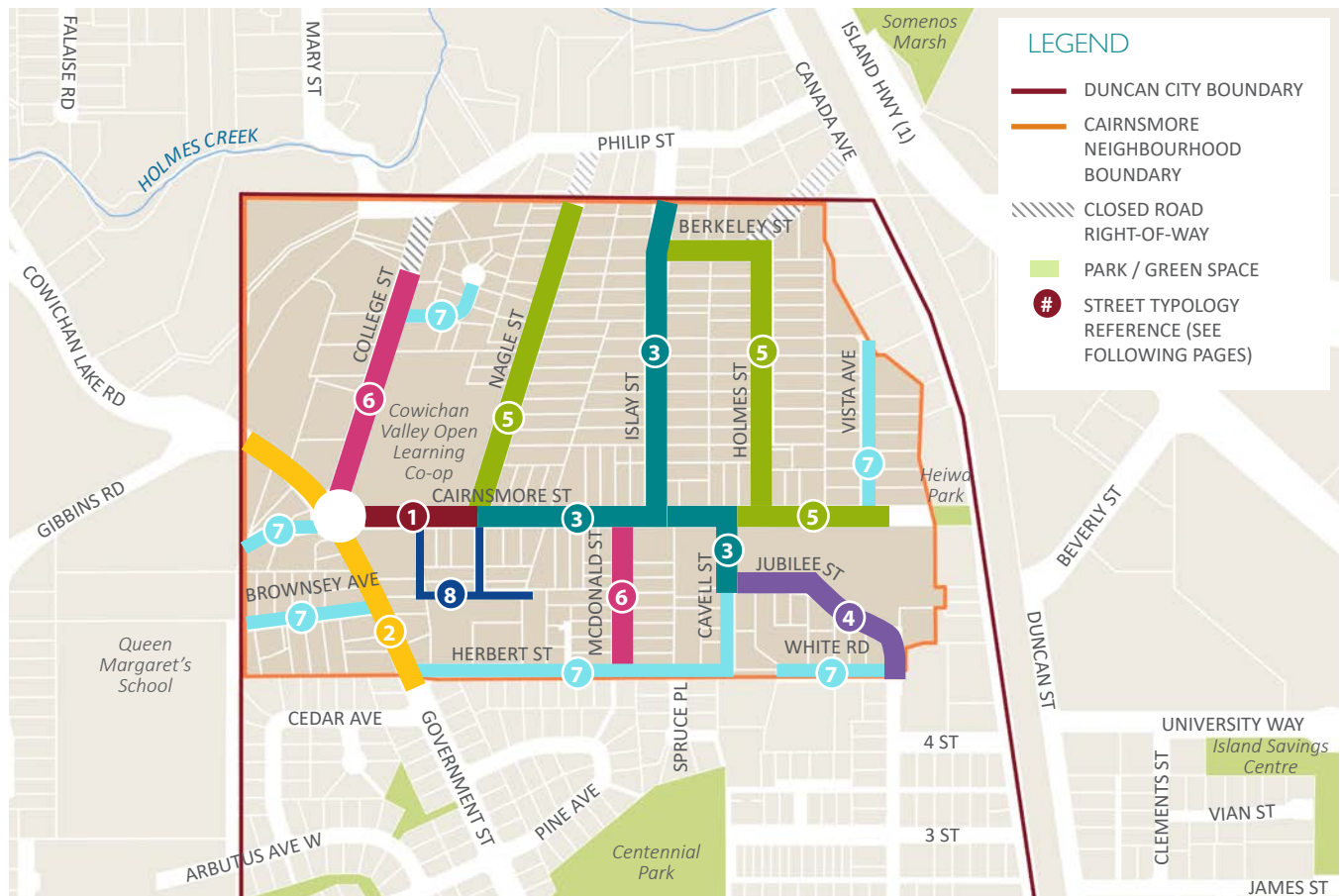


Complete streets are attractive and functional, providing an enjoyable and efficient experience as people move around the neighbourhood using whatever mode that works for them.

5.5.1 STREET TYPOLOGIES

- 1 The City shall integrate the streetscape elements identified in this section into the Transportation Master Plan. This plan provides guidance on desired elements. Details, including dimensions, will be in the Transportation Master Plan.

STREET TYPOLOGY OVERVIEW MAP



Map 20: Street Typologies in Cairnsmore

1 NEIGHBOURHOOD COMMERCIAL CORE – CAIRNSMORE STREET

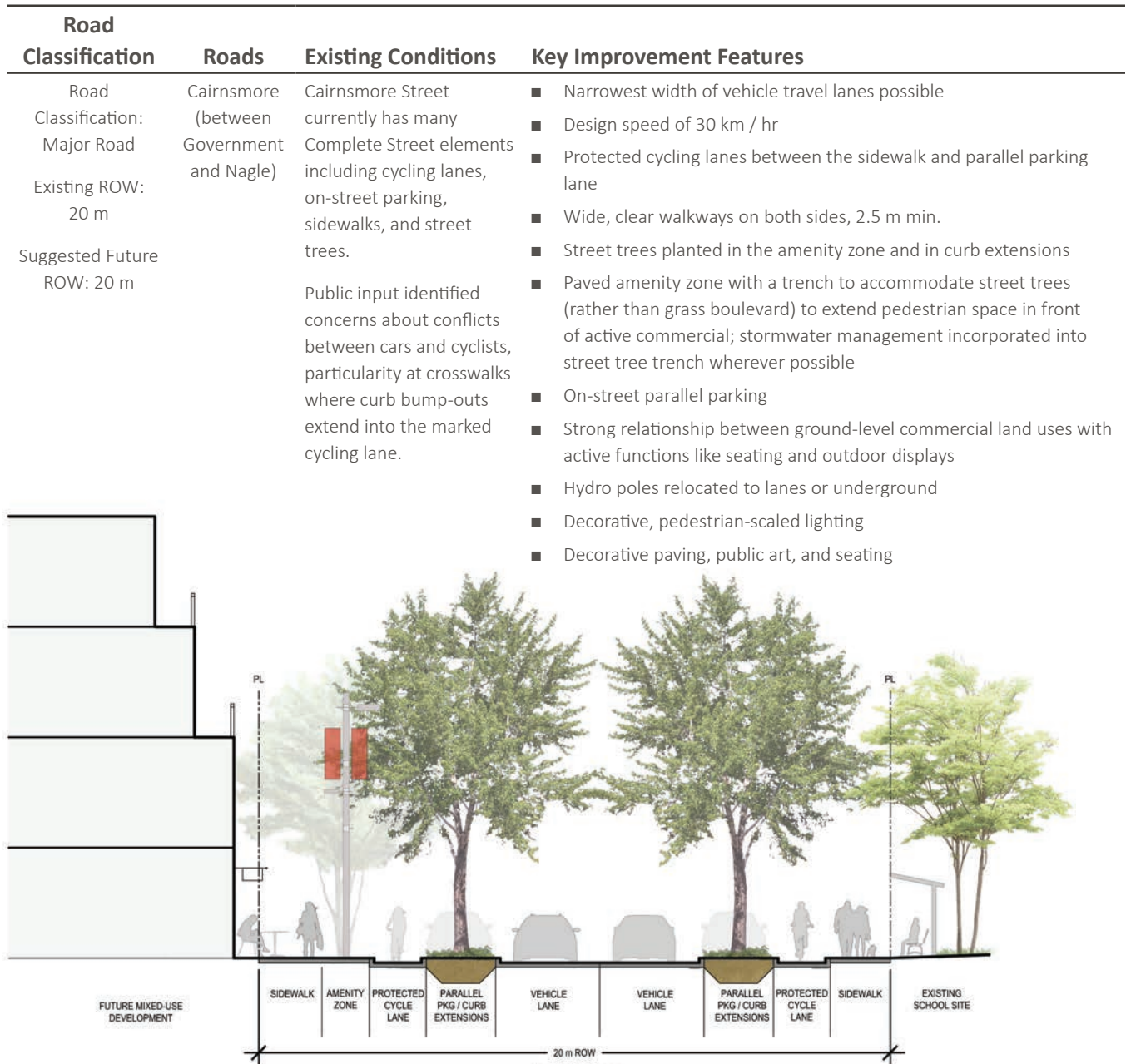


Figure 12: Typical Street Section for the Neighbourhood Commercial Core- Cairnsmore Street



TODAY: Cairnsmore Street at the neighbourhood commercial node is a more vehicle-dominated environment. Prioritizing pedestrian and cyclist comfort and safety is desired for the future.



EXAMPLE: Cairnsmore Street is envisioned to become a pedestrian- and cycling-friendly street with trees, an active pedestrian zone and commercial activities that flow into the street.



EXAMPLE: Protected cycling lanes on the curb side of parking buffer cyclists from moving cars.

2 NEIGHBOURHOOD COMMERCIAL CORE – GOVERNMENT STREET

Road Classification	Roads	Existing Conditions	Key Improvement Features
Road Classification: Major Road Existing ROW: 12.2 m to 16 m Suggested Future ROW: 20 m	Government (between Herbert and Gibbins)	Government Street right-of-way narrows through the Cairnsmore neighbourhood, making it more challenging to incorporate Complete Streets elements. The neighbourhood plan recommends securing additional right-of-way for Government Street (see Section 5.4).	<ul style="list-style-type: none"> ■ Narrowest width of vehicle travel lanes possible ■ Design speed of 30 km / hr ■ Protected cycling lanes with barrier between cyclists and vehicles ■ Walkways on both sides (2.5 m min. on neighbourhood commercial node side, 2.0 m min. on residential side) ■ Paved amenity zone with a trench to accommodate street trees (rather than grass boulevard) to extend pedestrian space in front of active commercial; stormwater management incorporated into street tree trench wherever possible ■ No on-street parking; signage should direct visitors to parking within the neighbourhood commercial node ■ Strong relationship between ground-level commercial land uses with active functions like seating and outdoor displays ■ Decorative, pedestrian-scaled lighting ■ Decorative paving, public art, and seating

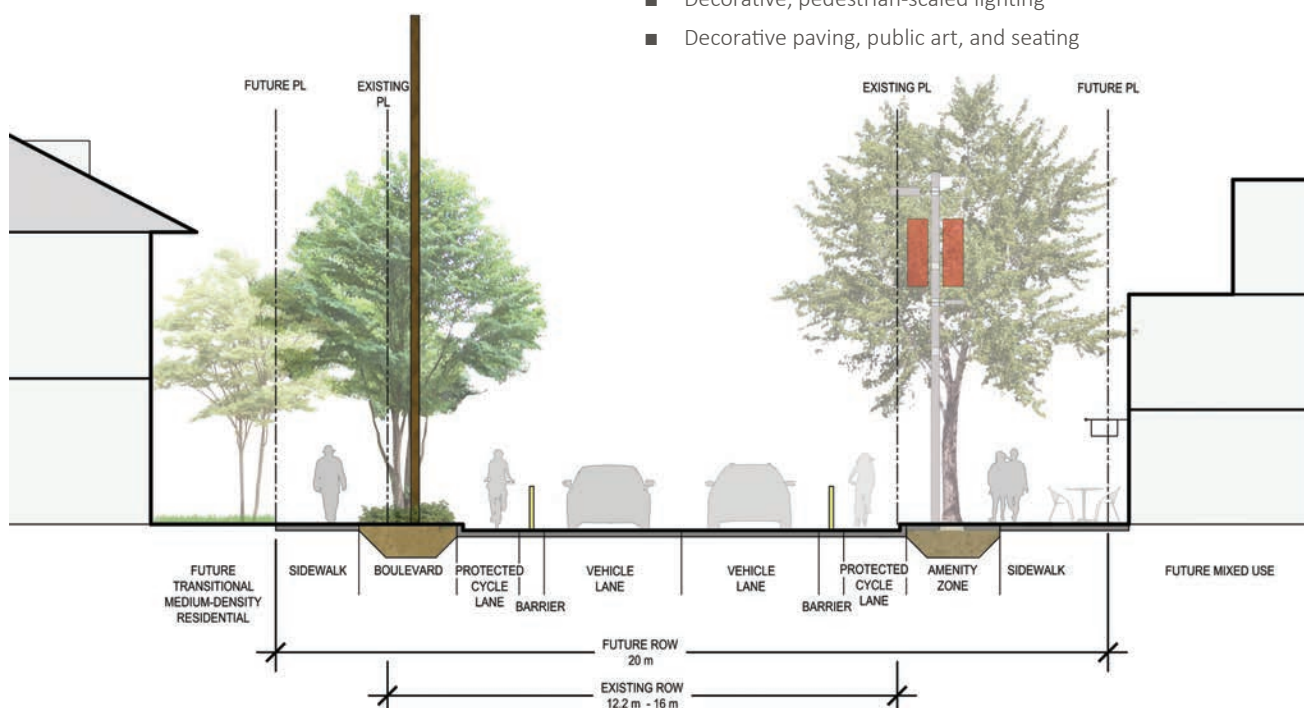


Figure 13: Typical Street Section for the Neighbourhood Commercial Core- Government Street



TODAY: Government Street right-of-way narrows through Cairnsmore leaving little room for Complete Street elements.



EXAMPLE: Protected cycle lanes provide a physical barrier between cyclists and moving vehicles.



EXAMPLE: Wide walkways with street trees provide for a pleasant pedestrian experience, even on a busy road.

3 MAJOR NEIGHBOURHOOD CONNECTOR (20 M RIGHT-OF-WAY)

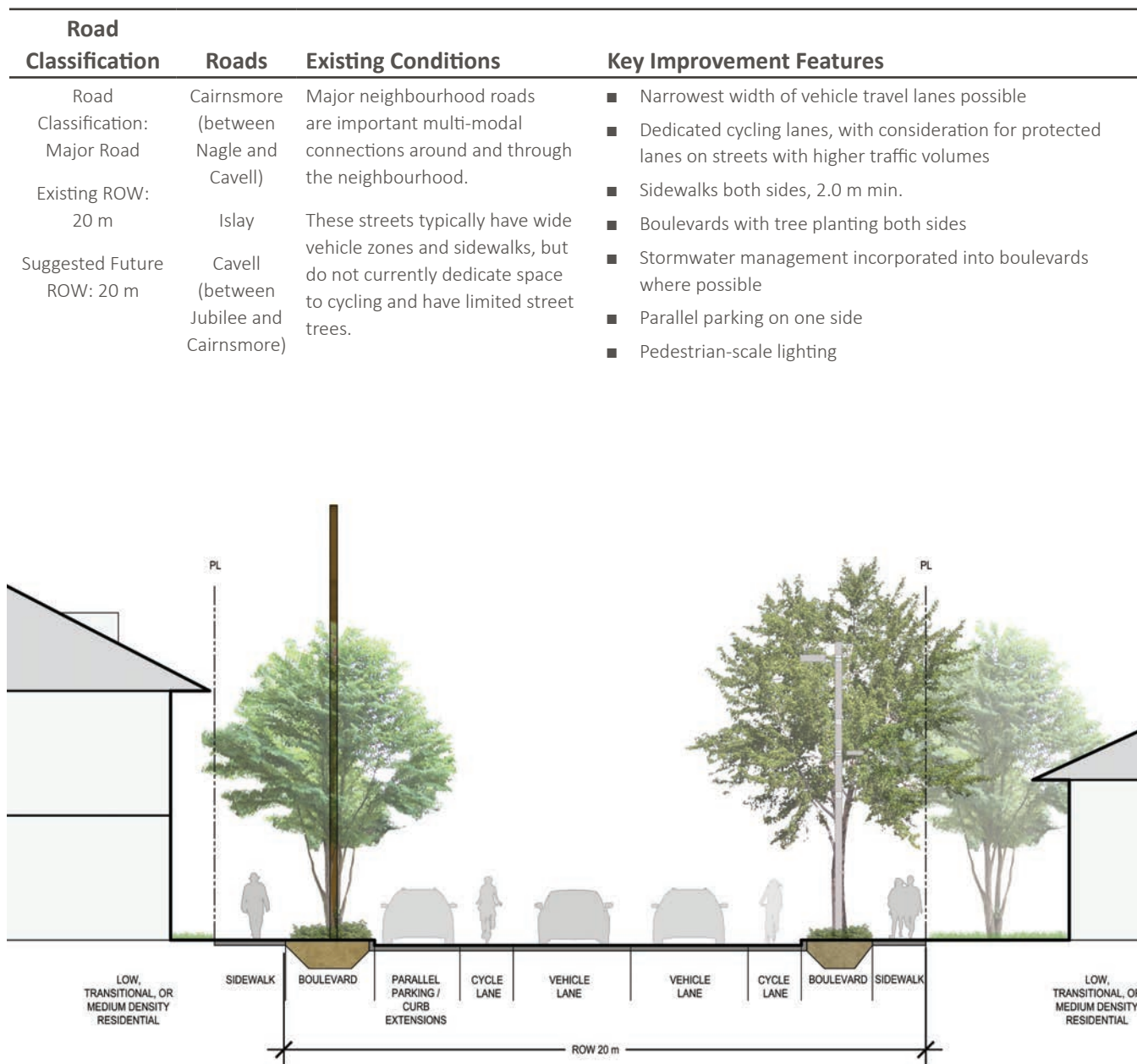


Figure 14: Typical Street Section for a Major Neighbourhood Connector



TODAY: Islay Street has a wide vehicle zone sidewalks and no street trees.



Photo: Payton Chung

EXAMPLE: A bicycle boulevard incorporates traffic calming measures and provides clearly marked cycling routes.



Photo: Brett VA

EXAMPLE: A swale provides rainwater management on a residential street.

4 MAJOR NEIGHBOURHOOD CONNECTOR (NARROW RIGHT-OF-WAY)

Road Classification	Roads	Existing Conditions	Key Improvement Features
Road Classification: Major Road Existing ROW: 12 m to 15 m Suggested Future ROW: 15m +	Jubilee	<p>Jubilee is classified as a major road and links Cairnsmore and Downtown Duncan, but has limited road right-of-way. As a result, pedestrian and cycling facilities on this route are constrained.</p> <p>The area is complicated by steep grades and limited visibility around the Jubilee Street corner. Public input identified concerns about cycling on this route.</p> <p>The neighbourhood plan recommends securing additional right-of-way for Jubilee Street (see Section 5.4).</p>	<ul style="list-style-type: none"> ■ Narrowest width of vehicle travel lanes possible ■ Design speed of 30 km / hr ■ Multi-use path, 4.0 m wide min. on the Cairnsmore Place side of Jubilee Road, separating cyclists and pedestrians from vehicle traffic ■ Pedestrian crossings on Cavell Street and Jubilee Street to access the trail ■ Sidewalk, 1.5 m min., on the downhill side of Jubilee Road for future access to adjacent residential uses ■ Relocate power poles outside cycling and pedestrian zones ■ Decorative, pedestrian-scaled lighting.

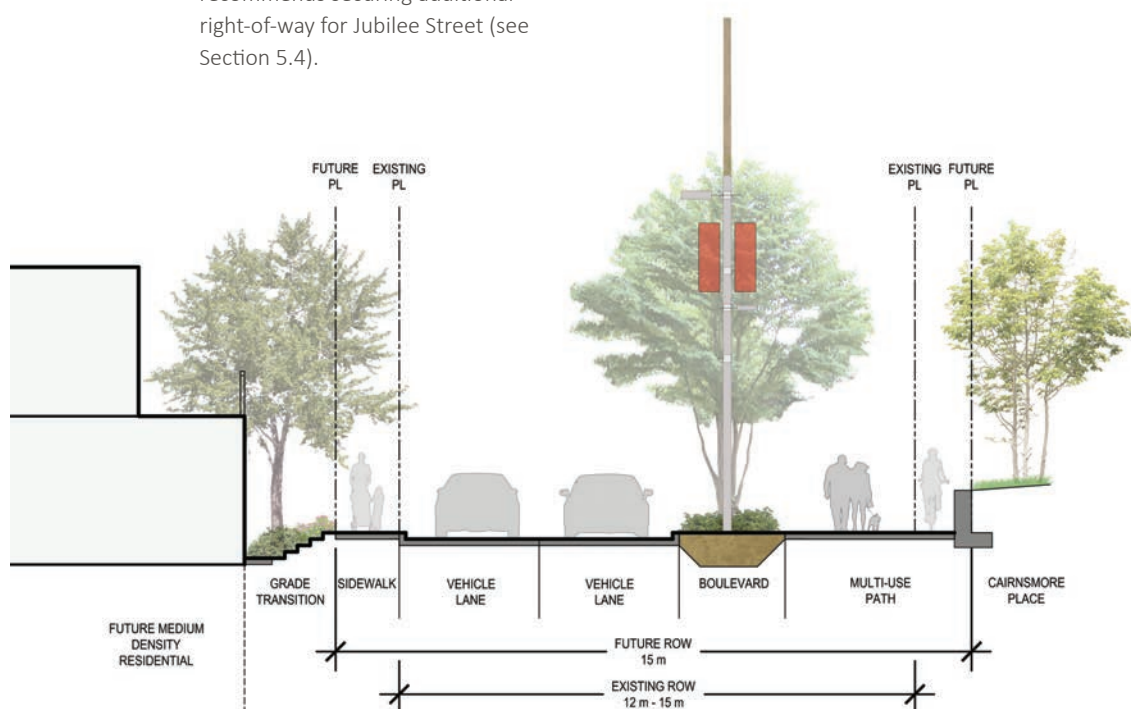


Figure 15: Typical Street Section for a Major Neighbourhood Connector (Narrow Right-of-Way)



TODAY: Jubilee Street has a narrow right-of-way and limited space on the hill near Cairnsmore Place. This area has been identified as a safety concern for cyclists.



EXAMPLE: An off-street multi-use path provides a safe, comfortable route for pedestrians and cyclists.



EXAMPLE: Marked routes for cyclists create clarity about travel routes.

5 LOCAL RESIDENTIAL STREET WITH BOULEVARD

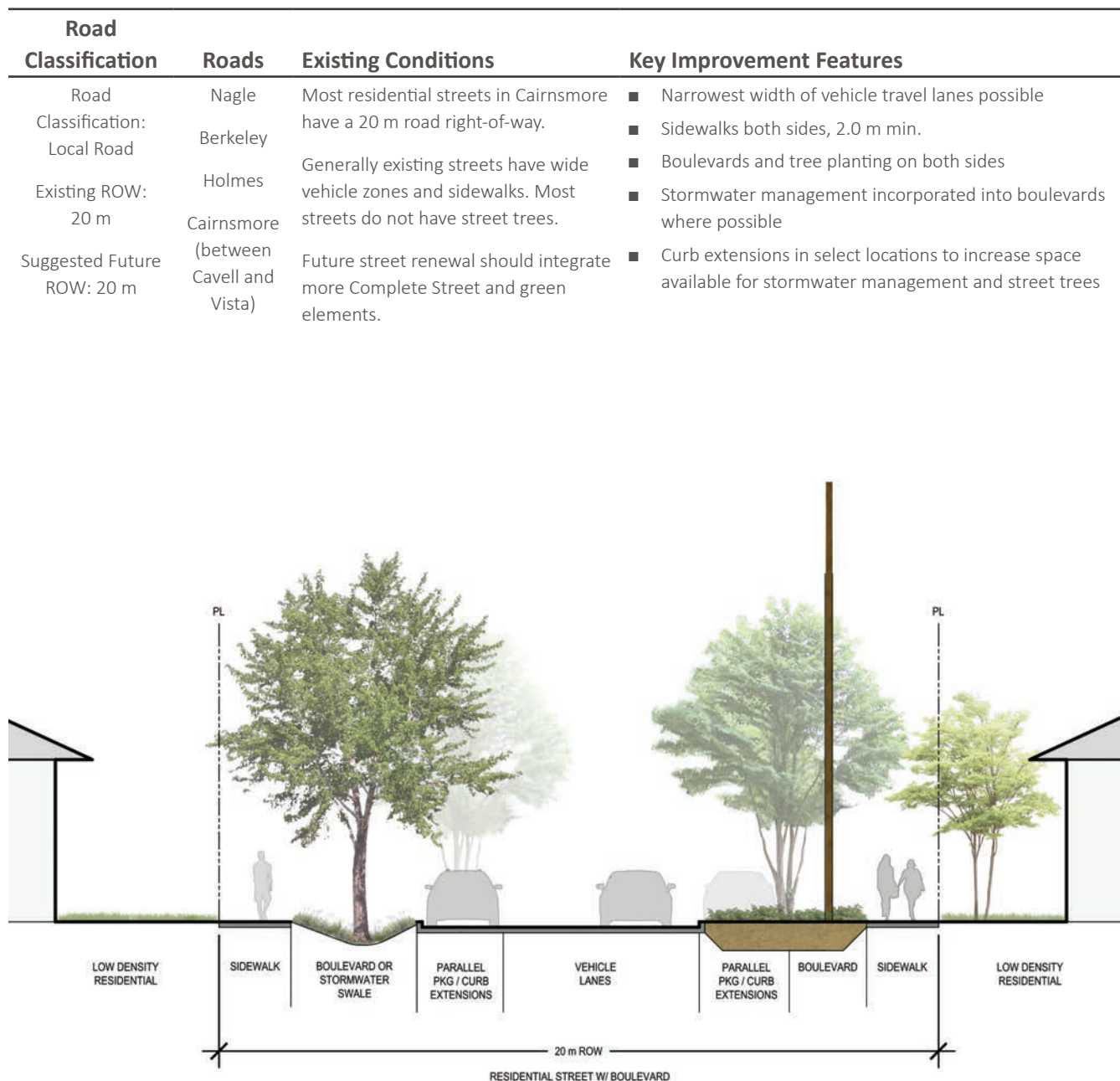


Figure 16: Typical Street Section for a Local Residential Street with Boulevard



TODAY: Nagle Street is a typical Cairnsmore residential street. The wide paved surface provides space for on-street parking, typically more than is required.



EXAMPLE: On low-volume residential streets, especially those without high demand for on-street parking, many communities are reducing pavement width to allow space for street trees and stormwater management components. An added benefit is traffic calming.



EXAMPLE: Traffic calming on residential streets make streets safer for all modes.

6 LOCAL RESIDENTIAL STREET WITH MEDIANS

Road Classification	Roads	Existing Conditions	Key Improvement Features
Road Classification: Local Road	MacDonald College	Most residential streets in Cairnsmore have a 20 m road right-of-way.	<ul style="list-style-type: none"> ■ Narrowest width of vehicle travel lanes possible ■ Sidewalks both sides, 2.0 m min. ■ Centre median with tree planting ■ Stormwater management incorporated into the median where possible ■ Curb extensions in select locations to increase space available for stormwater management features and street trees
Existing ROW: 20 m		Generally existing streets have wide vehicle zones and sidewalks. Most streets do not have street trees.	
Suggested Future ROW: 20 m		Future street renewal should integrate more Complete Street and green elements.	
		Certain streets have few driveways making a centre median feasible. A median can provide enhanced opportunities for street trees or stormwater functions, with fewer potential conflicts with utilities than boulevards.	

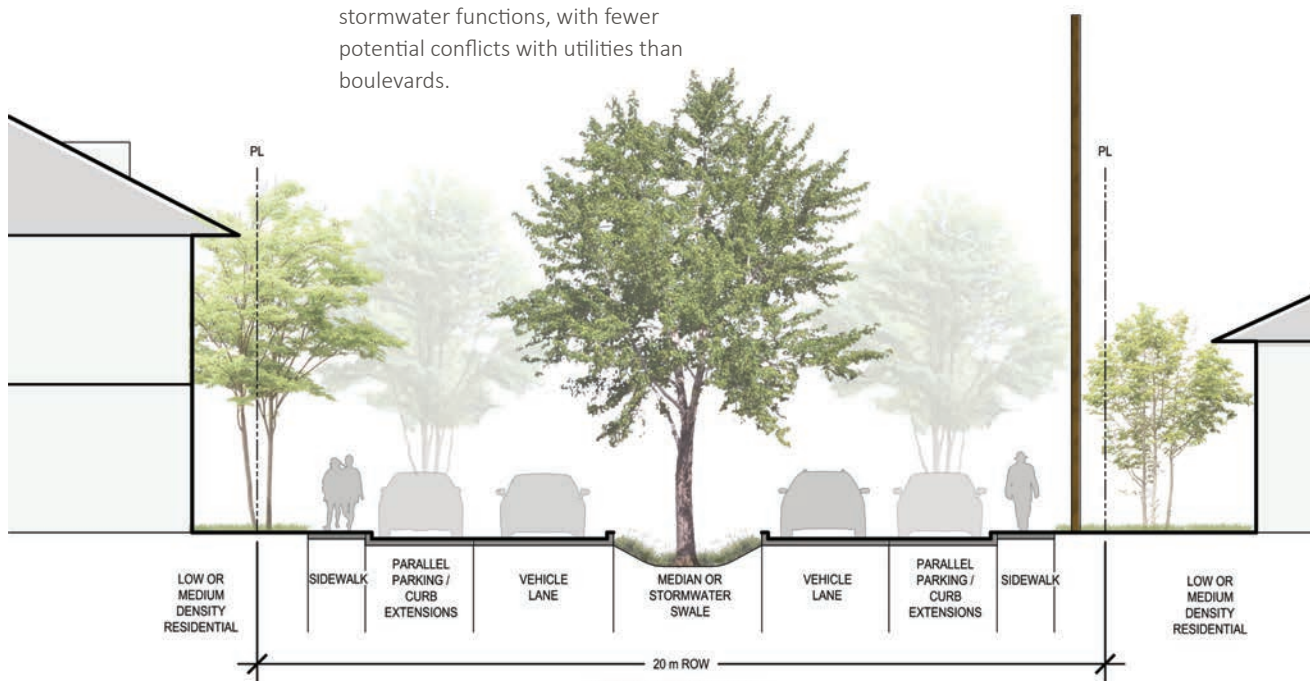


Figure 17: Typical Street Section for a Local Residential Street with Medians



TODAY: College Street currently does not have sidewalks or street trees.



EXAMPLE: Memorial Avenue in Qualicum Beach is considered a green street with rows of trees and on-street parking.



EXAMPLE: A central median along Island Highway in View Royal captures and treats stormwater from the roadway.

7 SHARED LOCAL STREETS

Road Classification	Roads	Existing Conditions	Key Improvement Features
Road Classification: Local Road	Herbert	Several residential streets in Cairnsmore have narrow road rights-of-way.	<ul style="list-style-type: none"> Retain existing roadway and parking (one side) Incorporate traffic calming elements such as chicanes or curb extensions to limit design speed to 20 km / hr; reduce cut-through activity, and provide space for street trees or stormwater management
Existing ROW: 10 m to 15 m	White	These streets are intended to carry local traffic only. These streets have either narrow sidewalks or all modes share the low volume roadway.	<ul style="list-style-type: none"> Sidewalks, where provided, 1.2 m min. Add signage indicating the road is for all users
Suggested Future ROW: Unchanged	Vista		
	Cavell (between Herbert and Jubilee)	Some concerns were identified about cut-through traffic using local roads (e.g., Herbert) to bypass busy areas.	
	Holmesdale		

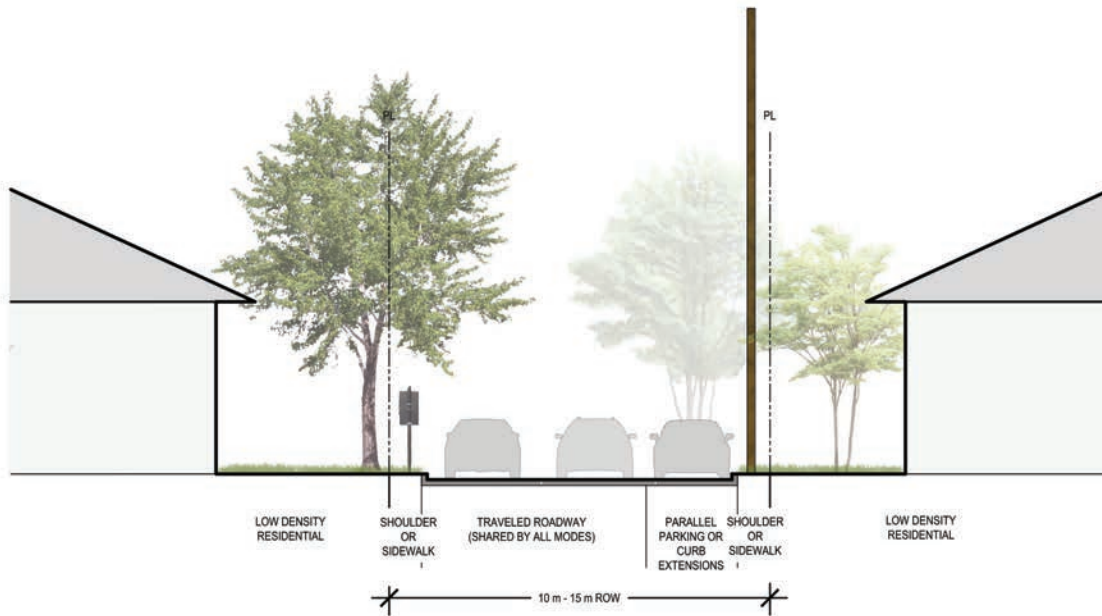


Figure 18: Typical Street Section for a Shared Local Street



TODAY: Herbert Street is typical of a low-volume street in Cairnsmore.



EXAMPLE: A sign indicating a shared roadway.



EXAMPLE: Chicanes used on a narrow shared street to provide traffic calming, while maintaining access for local residents and providing space for planting.

8 NEIGHBOURHOOD COMMERCIAL NODE SHARED LANES

Road Classification	Roads	Existing Conditions	Key Improvement Features
Road Classification: Lane Existing ROW: 4.5 m Suggested Future ROW: +/- 7 m	Lanes in the Neighbourhood Commercial Node	Gravel drive with no defined layout. This plan recommends using existing City-owned rights-of-way and securing new lane rights-of-way in the neighbourhood commercial node through agreement with adjacent owners or through the development application process (see Section 5.4).	<ul style="list-style-type: none"> ■ Multi-modal design to accommodate pedestrians, cyclists, vehicles, and service vehicles within the traveled surface ■ Design speed 20 km / hr or less ■ Alternative paving materials (pavers, patterned concrete) to indicate shared environment ■ Chicanes or other traffic calming features to slow vehicles and provide space for planting

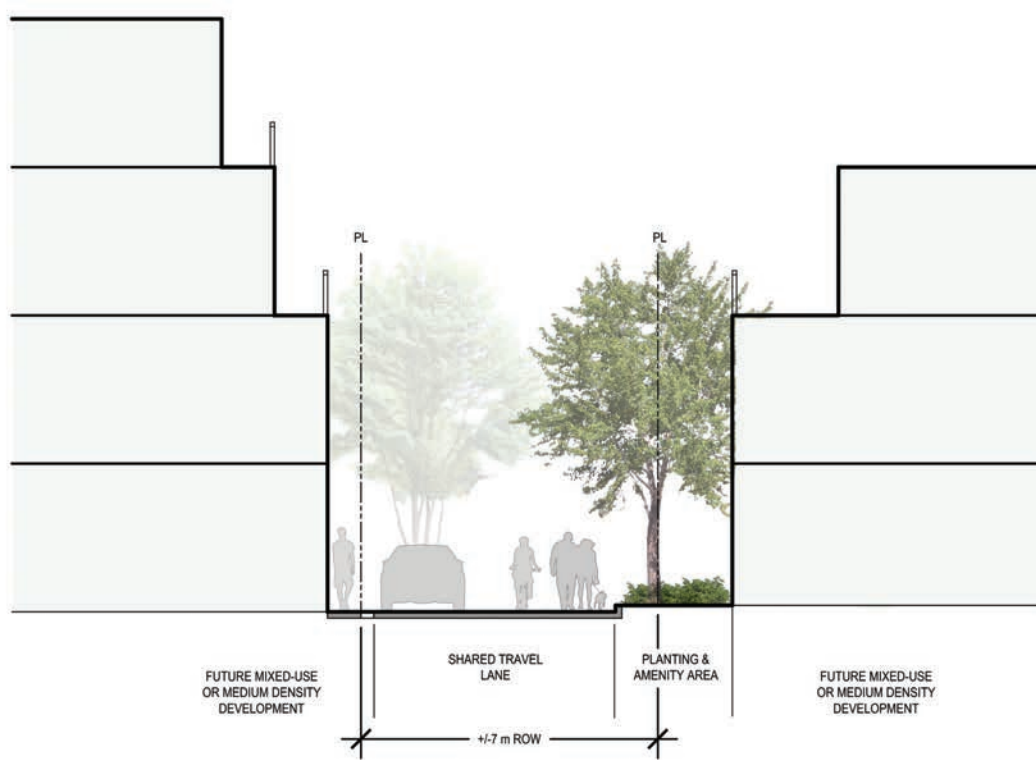


Figure 19: Typical Street Section for a Neighbourhood Commercial Node Shared Lane



TODAY: The existing gravel lane in the neighbourhood commercial node does not have defined circulation.



EXAMPLE: A shared urban lane in Germany uses textured pavers and chicanes to slow traffic speeds and provide an attractive environment.



EXAMPLE: A shared lane with a chicane goes past a multi-family residential building in Langford.

5.5.2 STREETSCAPE POLICIES

The streetscape can be logically divided into 4 zones characterized by the activities anticipated. Streetscape policies are organized around these zones.

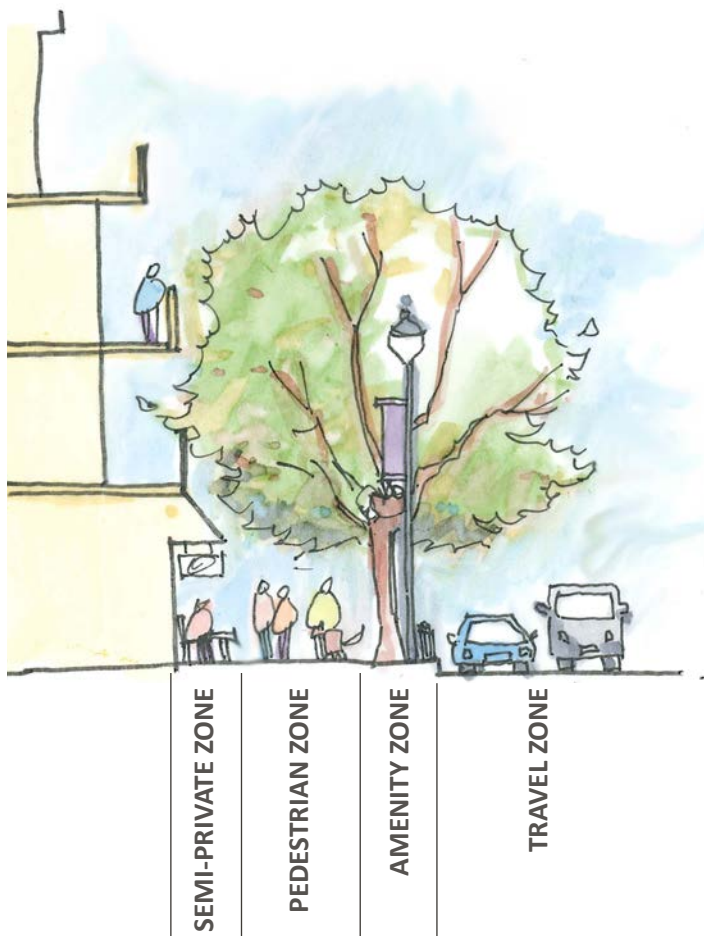


Figure 20: Streetscape Zones

SEMI-PRIVATE ZONE

- 1 Semi-private zones in front of buildings with ground-floor commercial shall be designed using materials and features that blend with the adjacent pedestrian realm and provide active uses such as seating or display of goods.



EXAMPLE: Outdoor restaurant seating in a semi-private zone.

PEDESTRIAN ZONE

- 2 Sidewalk location and width should be as outlined in Section 5.5.1 Street Typologies.
- 3 Sidewalks shall be continuous and paved with durable, even surfaced material that suits an urban residential neighbourhood (e.g., pavers or concrete, not asphalt or gravel).
- 4 Textured paving shall be used at intersections and crosswalks to support visual accessibility.



EXAMPLE: Textured crosswalk.

- 5 Pedestrian zone shall include a minimum 1.2 m wide pedestrian "clear" zone that provides uninterrupted, barrier-free access for all.



Photo: Sonny Abesamis

EXAMPLE: Streetscape with a pedestrian clear zone between the semi-private zone and amenity zone.

AMENITY ZONE

- 6 Seating and bicycle racks shall be provided at regular intervals throughout the neighbourhood commercial node and along all major streets.
- 7 Utility lines and poles within the neighbourhood commercial node shall be relocated:
 - » underground (preferred)
 - » on rear lanes
 - » on the side of the street opposite active commercial uses (not adjacent to active commercial uses)
- 8 Utility lines and poles in other parts of Cairnsmore shall be located to ensure 1.5 m clear distance in all walking areas.
- 9 Utility boxes shall be located to avoid inconvenience to the traveling public and designed to provide an attractive finish (e.g., painting, wrapping).
- 10 Decorative, pedestrian-scale street LED lighting shall be provided throughout the neighbourhood commercial node.
- 11 Public art, decorative surfacing, and wayfinding should be integrated into the amenity zone with an emphasis on the neighbourhood commercial node and major streets.
- 12 Large, full canopy trees should be used for all street trees (see Section 6.2).
- 13 Boulevards, medians, and curb extensions should be used for rain gardens and stormwater swales wherever feasible.



EXAMPLE: Rain garden (foreground) with seating and planters in the amenity zone.

TRAVEL ZONE

- 14 The narrowest vehicle travel lane width shall be used in all streets to support traffic calming and to maximize space for active transportation and amenities.
- 15 Traffic calming, such as curb extensions, traffic circles, or chicanes, shall be considered on local streets where traffic speed on cut-through issues are identified.
- 16 Where traffic calming is used, safe passage for bicycles shall be incorporated.
- 17 Bicycle facilities, including protection, shall be provided along streets indicated in Section 5.5.1.
- 18 Alternative surface treatments should be used for shared lanes in the neighbourhood commercial node such as permeable paving or textured concrete.
- 19 Driveway widths shall be limited to 3.0 m and clustered or shared driveways shall be strongly encouraged minimize interruption of pedestrian and cycling routes.
- 20 On-street parking should be incorporated into all streetscapes to reduce the need for off-street parking and provide a physical barrier between vehicular traffic and the sidewalk.
- 21 On-street parking stalls should be surfaced with permeable paving treatments and/or interspersed with rain gardens to support infiltration of rainwater. Decorative treatments within the neighbourhood commercial node should be strongly encouraged.



EXAMPLE: On-street parking using decorative pervious pavers, providing both attractive design and rainwater management.



Green spaces are at the heart of our neighbourhood experiences. Protecting and expanding Cairnsmore's green spaces is a priority for this plan.

6 | GREEN SPACES



More and more, green spaces are being recognized as vital components to quality of life. A well-developed green network protects nature, provides services like stormwater management, and promotes public health and well-being. This section provides guidance on protecting and expanding natural areas and the urban forest and future parks and open spaces in Cairnsmore.

IN THIS SECTION:

- General policies for green spaces
- An overview of the existing green network
- Guidance on natural areas and urban forest
- Guidance on parks and open space planning

6.1 OVERVIEW

This plan emphasizes the role of natural areas, trees, parks, and open spaces in contributing to neighbourhood character and providing functional ecological services. A primary focus is on protecting existing trees and natural areas while identifying opportunities for people to connect with green spaces throughout their daily routines.



The Canada Avenue stairs travel through a treed bank on the east side of Cairnsmore.

WHAT DO KEY DOCUMENTS SAY ABOUT GREEN SPACES?

INTEGRATED COMMUNITY SUSTAINABILITY PLAN:

The *ICSP Desired Outcomes for Aquatic and Terrestrial Natural Systems* suggests Duncan will protect and enhance local watersheds, old growth, and biologically diverse areas and will have strong mechanisms for protecting old trees and their ecosystems.

The *ICSP Desired Outcomes for Recreation & Leisure* suggests there should be parks throughout the community for rest, play, and growing food.

OFFICIAL COMMUNITY PLAN:

OCP Section 7.1: The Natural Environment includes policy for both regulatory and incentive-based tree protection and encourages planting trees on public and private lands.

OCP Section 9.6: Parks, Open Spaces, and Recreation includes policies that encourage an interconnected open space system that respects the natural environment and provides opportunities for a diversity of needs and abilities.

OCP Section 12: Development Permit Areas includes Development Permit Areas to protect indigenous species, raptor habitat, aquifers, riparian areas, steep slopes, and flood plains.

The OCP identifies steep slope hazard lands in the northwest and east parts of Cairnsmore. Any future development in these areas is subject to *Development Permit Area 6 - Hazard Lands Development Permit Area Guidelines*.

URBAN FOREST STRATEGY:

The vision indicates that the City will invest in its urban forest and actively encourage public participation in its protection and enhancement. The strategy sets an average long-term City-wide target of achieving 40% tree cover by 2050.

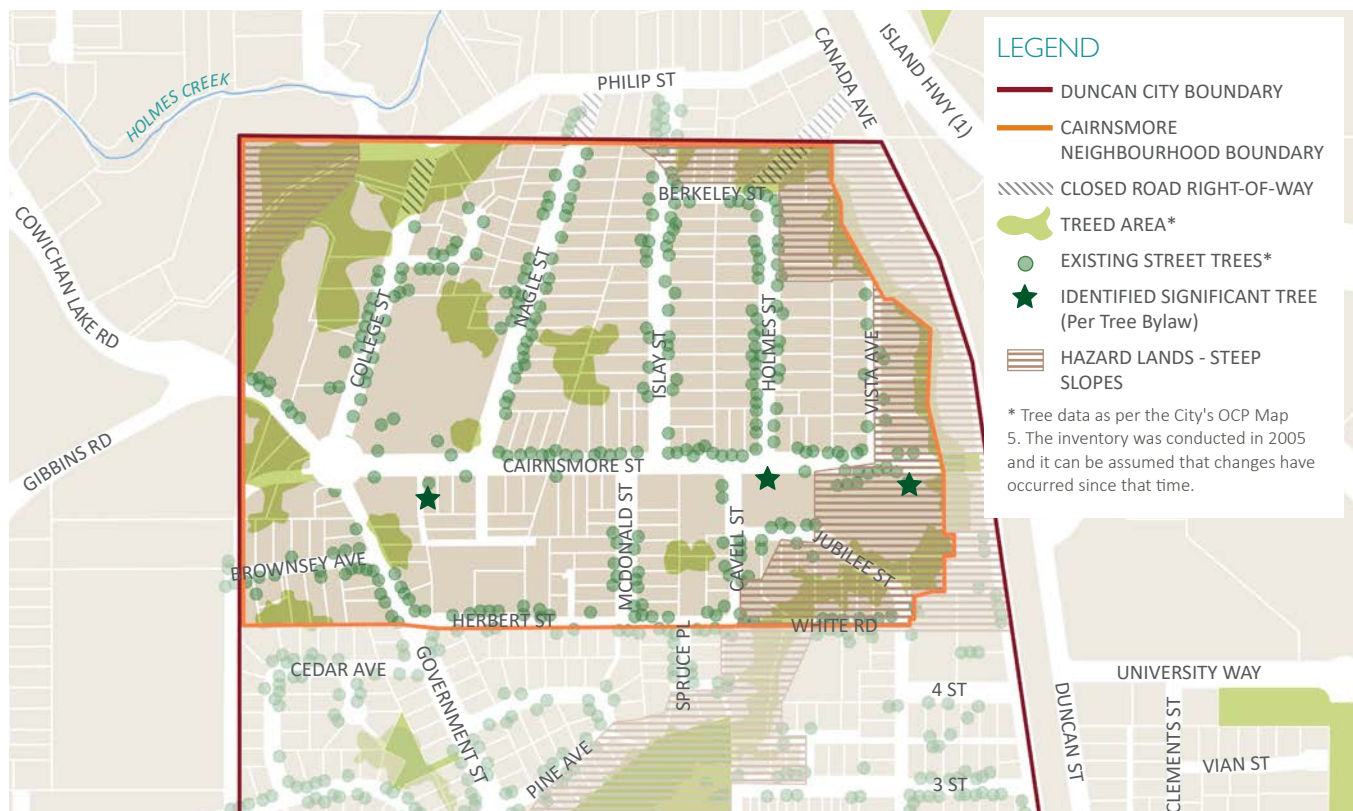
TREE PROTECTION BYLAW NO. 3135:

The bylaw defines "significant trees" and "protected trees" based on size, species, and function and requires landowners to obtain a valid permit prior to cutting or removing any tree that is a "significant tree" or meets the criteria of a "protected tree." Many existing mature trees in Cairnsmore meet these criteria.

6.2 NATURAL AREAS & URBAN FOREST

Natural areas, including creeks, woodlands, riparian areas, and wetlands, are important assets. Once lost it is difficult to redevelop or recreate natural areas – a process that only occurs with the input of time and natural processes. Because Cairnsmore is largely developed, existing natural areas are primarily wooded areas on banks along the eastern boundary and in the northwest corner. Preserving existing wooded areas should be a priority.

Mature trees contribute to neighbourhood character and backdrop. In Cairnsmore, the majority of mature trees are on private land. Trees mature over many decades, becoming increasingly valuable with age and it has been speculated that with climate change, it may not be possible to regenerate the mature trees we have today due to droughts or extreme weather cycles. The neighbourhood plan emphasizes the importance of retaining existing trees and building in succession for the future.



Map 21: Existing Natural Areas & Urban Forest



EXISTING: Streets in Cairnsmore typically have a lot of right-of-way dedicated to the paved roadway, some of which could be reallocated to space for expanding streets trees.



EXAMPLE: Allocating road right-of-way to street trees enhances environmental and aesthetic benefits.

NATURAL AREAS & URBAN FOREST POLICIES

GENERAL

- 1 The City should set a tree cover target of 45% for the Cairnsmore neighbourhood in support of the City-wide target of 40% tree cover recommended in the Urban Forest Strategy.
- 2 Existing wooded areas should be protected with the goal of maintaining and enhancing biological diversity.
- 3 Tree planting shall consider the following:
 - » New trees should be as listed in the City's Urban Forest Strategy.
 - » Large trees that will provide more substantial canopy at maturity are preferred.
 - » Adequate soil volumes will be provided to ensure tree health and development.
- 4 The City should consider developing a policy that specifies a calculation method or standard for appropriate tree soil volumes.
- 7 The City should support reasonable variances for development proposals in order to retain and protect existing mature trees.
- 8 The City should work with institutional land owners to increase trees on their properties.
- 9 The City should support and develop programs, potentially in partnership with external organizations, to encourage and incentivize private landowners to plant trees on private properties to help support ongoing and long-term renewal of the large-scale tree canopy.
- 10 The City should provide resident education about opportunities to create biodiversity within their private spaces.
- 11 The City should encourage and support orchard development in partnership with organizations. Candidate sites may include:
 - » Duncan Elementary
 - » Girl Guide Hall
 - » St. Andrews Church
 - » Cairnsmore Place

TREES ON PRIVATE LANDS

- 5 The City should discourage removal of existing mature trees for reasons other than hazard or safety. Where tree removal is proposed, the City shall require an adequate replacement strategy.
- 6 The City should require tree replacement at the following rates:
 - » Small / medium trees lost to natural causes: 1:1 min.
 - » Large trees lost to natural causes: 2:1 min.
 - » Trees removed for other purposes: 2:1 min.

If trees cannot be accommodated on site, new trees should be planted in a suitable public location selected by the City.

TREES ON PUBLIC LANDS

- 12 The City should prioritize planting street trees in road rights-of-way, with the goal of establishing continuous treed canopies along all streets.
- 13 Where new parkland is secured, maintain existing mature trees wherever possible.
- 14 The same species of street trees should be used within blocks to create a consistent form. Species should vary block by block to encourage diversity.
- 15 Street trees planted in hardscape areas shall be planted in tree wells or trenches that provide adequate soil volume.

A GOOD EXAMPLE: The Town of Richmond Hill, ON, in partnership with the York Region and the Local Enhancement and Appreciation of Forests (LEAF), developed the LEAF Backyard Tree Planting Program to provide trees to residents at a subsidized rate. The cost of the tree includes site consultation with a tree specialist, one tree, and full planting service. Since 1996 LEAF has planted more than 17,000 native trees and shrubs in residential backyards.





A GOOD EXAMPLE: PLAZA SPACES BETWEEN BUILDINGS

The spaces between buildings can be important components of a neighbourhood. These are the places where informal meetings occur, people catch up or just relax. These spaces don't need to be necessarily large or complex – the ingredients of shelter, seating, light, and green are often all that is needed to create a compelling space.

The Fitzwilliam Gate, on Fitzwilliam Street in Nanaimo, tucks a narrow pedestrian greenway beneath a glass atrium. Visitors drink coffee at the tables and chairs or browse shop displays that spill out into the welcoming plaza.

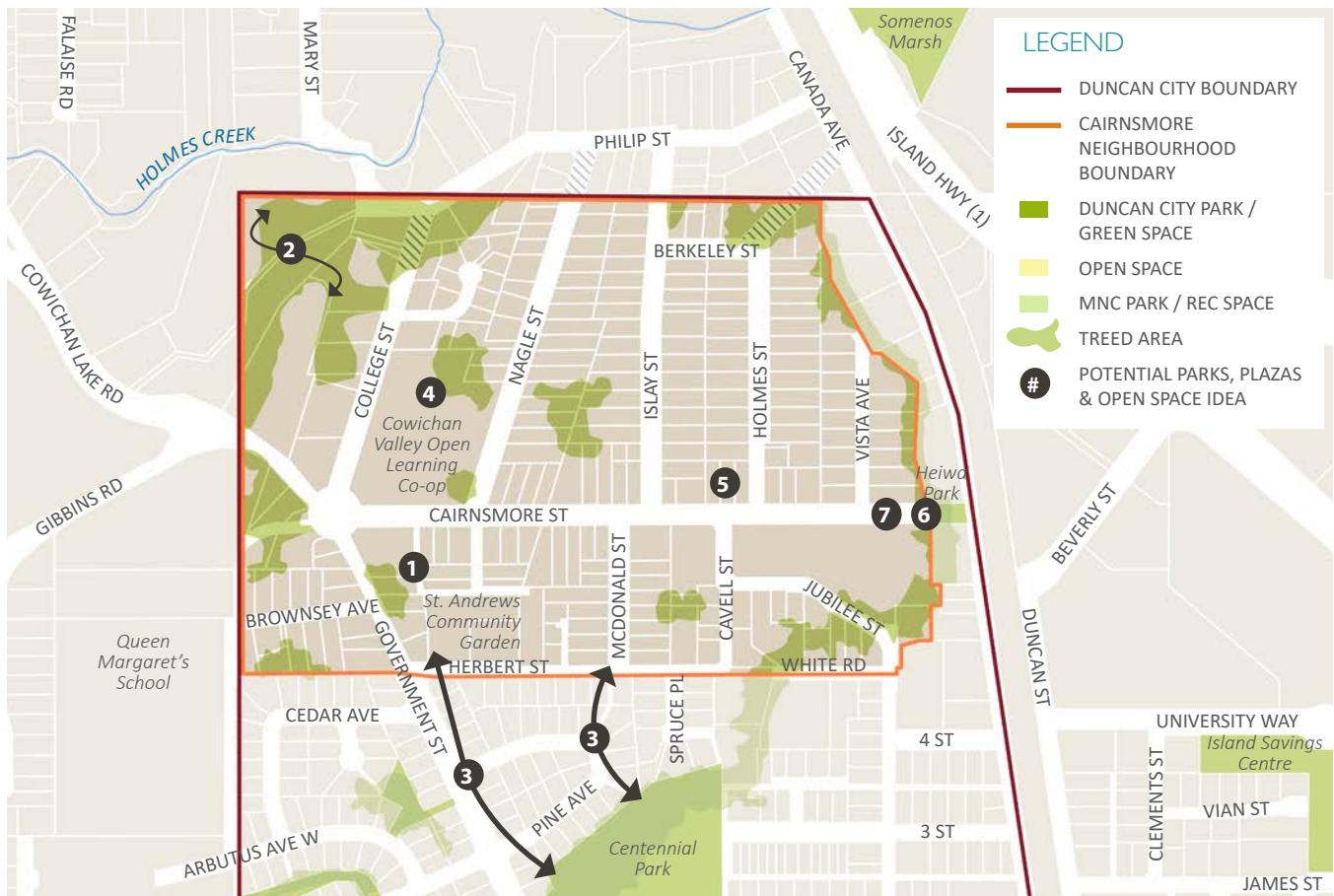
6.3 PARKS, PLAZAS, & OPEN SPACES

Today there are no existing formal parks or plazas within the Cairnsmore neighbourhood. A number of open spaces around institutional uses including the field and Duncan Elementary, St. Andrews Church community garden, and the area at Guide Hall are used by the neighbourhood.

While it's anticipated that Cairnsmore residents will continue to travel to major recreation destinations like Centennial Park or the Cowichan Community Centre, the City has a Development Cost Charge for this area

to increase park space and this neighbourhood plan provides policies on desired park and open space in Cairnsmore.

It is envisioned that parks, plazas, and open spaces will be integrated into Cairnsmore over time. A number of directions are provided in this plan; however, as new development or redevelopment occurs, partnerships are formed, or City-led initiatives or funding opportunities arise, the location and character of parks, plazas, and open spaces may evolve.



Map 22: Parks & Open Spaces



TODAY: The field space by Duncan Elementary (CVOLC) provides neighbourhood open space.



EXAMPLE: Spaces for families and friends to informally gather and relax would add to the feeling of neighbourliness.

PARKS, PLAZAS & OPEN SPACES POLICIES

- 1 The future neighbourhood commercial node shall include the following public spaces:
 - » One new park space with amenities including seating, play features, and formal and informal gathering spaces, at least 1,000 square metres in size.
 - » One or more public plaza spaces with weather-protected seating, vegetation, and art.
- 2 The City should secure natural park space in the northwest part of the neighbourhood with the purpose of protecting existing natural area and supporting an active transportation connection (see Section 5.2) at the time of development application or through alternative means.
- 3 The City should improve connection to Centennial Park by adding new multi-use trails and wayfinding.
- 4 The City should work with School District #79 and the Cowichan Valley Open Learning Co-op to support public use and enjoyment of open spaces around Duncan Elementary for both students and neighbourhood residents. Ideas include:
 - » Playing field improvements
 - » Community gardens
 - » Dog recreation
 - » Children's play
 - » Improved basketball courts
 - » Welcoming plaza space adjacent to Cairnsmore Street
 - » Space for rest and relaxation
- 5 The City should work with Girl Guides to support neighbourhood use and enjoyment of the open space around Girl Guide Hall.
- 6 The City should improve Canada Avenue staircase (see Section 5.2).
- 7 The City should convert the unused portion of road right-of-way at the end of Cairnsmore Street to public open space.
- 8 New public parks shall be acquired or legally secured through the development or land use application processes in accordance with the policies of this plan.
- 9 The City should prioritize provision of the following amenities in the neighbourhood:
 - » Children's play space
 - » Additional community gardens
 - » Tranquil green spaces
 - » Seating areas
 - » Trails
 - » Off-leash dog area
 - » Orchard
- 10 Design of parks, plazas, and open space shall include the following considerations:
 - » Adherence to Crime Prevention Through Environmental Design (CPTED) principles
 - » Lighting on primary pedestrian routes in parks and open spaces
 - » Accommodation of all ages and abilities
 - » Support of wildlife habitat, green stormwater management, and urban forest development
 - » A focus on planting native and adaptive plant species and removal of invasive species
 - » Provision of logical active transportation connections
 - » Provision of high-quality amenities that make the spaces enjoyable and functional for visitors such as seating, bike racks, and wayfinding



The Cairnsmore neighbourhood already has a close-knit feeling that should be protected and enhanced in the future.

7 | NEIGHBOURHOOD WELL-BEING



Cairnsmore is a neighbourhood where people stop to talk on the street. Maintaining and growing this sense of community is important to the intangible element that makes a neighbourhood feel like home to all who reside within it.

IN THIS SECTION:

- Policies on inclusiveness and accessibility
- Policies on community safety
- Policies about arts and social spaces

7.1 OVERVIEW

This plan encourages programming and partnerships to help maintain a caring and inclusive Cairnsmore. A vibrant neighbourhood creates a strong sense of place and provides diverse opportunities for living, working, and connecting.



The existing totem at the corner of Cairnsmore and Government Streets.

WHAT DO KEY DOCUMENTS SAY ABOUT NEIGHBOURHOOD WELL-BEING?

INTEGRATED COMMUNITY SUSTAINABILITY PLAN:

The *ICSP Desired Outcomes for Community and Individual Health* identifies Duncan as a community where people can age in place and where there is a strong sense of community.

OFFICIAL COMMUNITY PLAN:

OCP Section 8.1: Distinct and Diverse Neighbourhoods encourages land uses, amenities, and social practices, such as creation of Neighbourhood Associations, to forge connections and build neighbourhood identity.

OCP Section 8.3: Arts and Culture recommends a public art program for all public areas, new commercial, institutional, and multi-family development.

OCP Section 9.7: Education encourages collaboration with the School District to provide effective, reciprocal, and integrative use of municipal and school facilities, land, and programs that will maximize the community benefit of public resources.

PUBLIC ART POLICY:


The City has a policy that supports public art in two main ways:

1. Public Art - placement and maintenance;
2. Private Sector Public Art - incorporation of artistic elements and high quality design in new developments.

The policy provides a process for selecting public art.

AGE-FRIENDLY SENIORS SAFETY REPORT:

Recognizing that Duncan's senior population is considerably higher than the rest of the province, the report provides recommendations on creating more age-friendly buildings, spaces, and networks.



An inclusive community is one that removes barriers and welcomes everyone.

7.2 INCLUSIVENESS & ACCESSIBILITY

An inclusive and accessible neighbourhood is one where its citizens and members feel safe, respected, and comfortable in being themselves and expressing all aspects of their identities. It is a place where each person shares a sense of belonging with its other members.

INCLUSIVENESS & ACCESSIBILITY POLICIES

- 1 All business operations, community destinations, patios, and plazas should have barrier-free access.
- 2 All pedestrian environments should include:
 - » Sidewalks free of obstructions and with curb ramps at appropriate locations
 - » Accessible parking spaces in both public and private spaces
 - » Tactile signage and surface markings
 - » Audible pedestrian crossing signals
- 3 The City should encourage a range of non-market and market housing options to support a diverse and inclusive community.
- 4 The City should encourage developers to consider adaptable housing that supports changing needs through a person's life cycle. Examples include units that could convert from a family bedroom / den to a rental suite or kitchens / bathrooms that can be converted to allow aging in place.
- 5 The City should encourage continued delivery of childcare and eldercare services within Cairnsmore and support further development of these functions, provided they align with their surroundings.
- 6 The City should work with neighbourhood citizens to identify volunteer opportunities in the neighbourhood.
- 7 The City should encourage interaction between residents of Cairnsmore Place and the future Hospice and neighbourhood residents through invitation to community events, establishment of outdoor spaces near Cairnsmore Place that provide opportunity for social interaction, and ongoing communication with facility leadership to identify opportunities to increase interaction.
- 8 The City should consider developing a grant program for citizen-led neighbourhood activities or initiatives.



A neighbourhood that feels safe, night or day, is important for neighbourhood well-being.

7.3 COMMUNITY SAFETY

Feeling safe within your neighbourhood is fundamental to quality of life. Social challenges including theft, homelessness, and drug use exist and many of these issues will continue to be addressed at the City and regional scales. At a neighbourhood scale, planning built form and services to support an increased feeling of safety in the neighbourhood is important.

COMMUNITY SAFETY POLICIES

- 1 The City shall require all new development applications to provide details about how the project will meet CPTED (Crime Prevention Through Environmental Design) principles.
- 2 Lighting shall be integrated into major streetscapes and active transportation routes to provide visibility.
- 3 The City should support existing Block Watch groups and help facilitate establishment or connection of new groups to the network.
- 4 The City should educate residents on reporting potential criminal activity in the area.
- 5 The City should work with property owners to encourage redevelopment or removal of unused or abandoned properties.



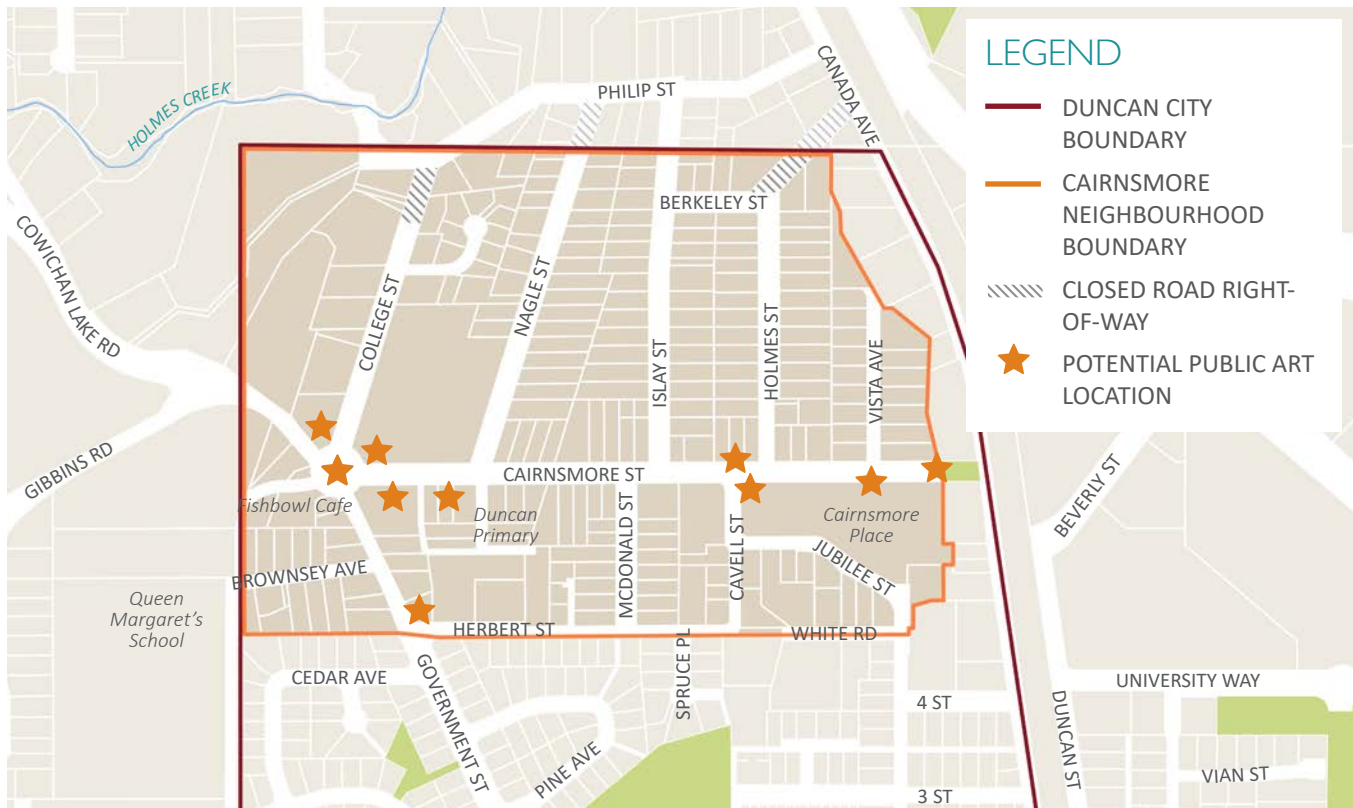
Well-lit walking routes with good passive surveillance are important to creating a feeling of safety.



Cairnsmore is fortunate to have limited run down properties at this time. Properties that have been damaged or run down can impact the character of the neighbourhood.

7.4 ARTS & CULTURE

A vibrant community weaves arts and culture into everyday life, helping to create a strong sense of place. Opportunities for creative placemaking include temporary and permanent public art opportunities and performance spaces in parks and other public spaces.



Map 23: Potential Public Art Locations in Cairnsmore

ARTS & CULTURE POLICIES

- 1 The City should collaborate with Cowichan Tribes to identify opportunities for art within the neighbourhood.
- 2 The City should support citizens in developing a Cairnsmore identity that is visibly demonstrated throughout the neighbourhood in signage, banners, art, furnishings, lighting, etc.
- 3 Public art shall be prioritized in the following locations:
 - » Places indicated on Map 23
 - » Streetscapes
- 4 Public art may be considered as part of a community amenity contribution provided it aligns with the City's public art policy and includes provisions to ensure long-term maintenance or replacement of the art.
- 5 The City should support volunteers or arts organizations to plan and implement artistic improvements or installations and events such as temporary pop-ups or seasonal celebrations.

EXAMPLES OF NEIGHBOURHOODS PROMOTING WELL-BEING

A GOOD EXAMPLE: ESQUIMALT TOGETHER AGAINST GRAFFITI (ETAG)

Esquimalt police department, staff, and community volunteers work together to clean-up existing graffiti and respond to new instances of vandalism. The program provides residents with a phone line to call and report graffiti so it can be removed swiftly, reducing the ongoing degradation of public spaces.



Photo: Tofino-Ucluelet Westerly News

A GOOD EXAMPLE: PREPAREDNESS PARTY, UCLUELET, BC

Ucluelet residents held an emergency preparedness-themed block party and barbecue to get to know one another and discuss how to be prepared if disaster strikes. The party included discussion about emergency kits, evacuation strategies, and general preparedness, as well as food. By taking time to get to know neighbours, the community is better prepared to respond if something happens.

A GOOD EXAMPLE: PONG IN THE PARK BY THE URBAN CONGA, TAMPA, FL

The Urban Conga, a design firm promoting community activity and social interaction through play, created the Pong in the Park project to activate public spaces in downtown Tampa, a place with little street activity. They created and installed durable, colourful tables and worked with local businesses to provide paddles and balls for the tables, connecting local businesses with activity.



Photo: The Urban Conga
(www.theurbanconga.com)



The City envisions a stormwater system that reduces pressure on underground pipes by managing a portion of rainfall on site through infiltration.

8 | SUSTAINABLE UTILITIES & SERVICES



Utilities and services are the bones of a neighbourhood – mostly hidden from view beneath the ground, but vitally important for keeping the neighbourhood alive and operating. The water, sanitary, stormwater, and solid waste systems are key to sustainability of the city and the health and well-being of the people who live here.

IN THIS SECTION:

- Policies on sustainable utilities and services

8.1 OVERVIEW

With early development dating back to the turn of the 20th century, Cairnsmore has a mix of both older and modern infrastructure.

Maintaining and updating Cairnsmore's utilities and services will be essential to meeting the City's sustainability goals, supporting modest growth, and helping ensure residents' health and well-being.



Underground utilities under construction.

WHAT DO KEY DOCUMENTS SAY ABOUT SUSTAINABLE UTILITIES & SERVICES?

INTEGRATED COMMUNITY SUSTAINABILITY PLAN:

The *ICSP Desired Outcomes for Buildings and Sites* identifies that Duncan will have replaced older water and sewer systems and plants throughout the City with green infrastructure.

OFFICIAL COMMUNITY PLAN:

OCP Section 10.1: Servicing identifies policies around monitoring of existing infrastructure, provision of efficient services, use of DCCs, and planning for replacement.

OCP Section 10.3: Rainwater Management suggests that the City's stormwater collection system is currently adequate, but it may not be sufficient to handle increased growth without instituting

alternative drainage systems that deal with the peak flow (first 30 minutes) of rainwater on-site, rather than relying solely on the stormwater pipes. The OCP requires developers to provide a drainage plan of the area proposed for development to address the effective and environmentally sensitive handling of peak flows.

ASSESSMENT OF UNDERGROUND INFRASTRUCTURE CONDITION:

The City has been completing assessments of existing underground infrastructure to understand and prioritize logical maintenance and renewal of the system.

8.2 SUSTAINABLE UTILITIES & SERVICES

- 1 As part of ongoing City-wide underground infrastructure assessments, the City shall assess existing water, sewer, and stormwater networks in Cairnsmore and identify upgrades that may be necessary for renewal and to realize the directions in this plan.
- 2 The City shall incorporate the directions of this neighbourhood plan into its infrastructure improvement plans which will upgrade and replace infrastructure according to capital plans, budgets, and Council approval. The timing of improvements is subject to change.
- 3 Proponents and land developers shall be responsible for providing the necessary upgrades resulting from a development or land use application in accordance with the policies of this plan.
- 4 The City should facilitate infrastructure and servicing upgrades through latecomer or excess capacity agreements where applicable through legislation.
- 5 The City should identify opportunities to incorporate green stormwater infrastructure or "green streets" as part of all utility, active transportation, or other street improvements (see Section 5).
- 6 The City shall pursue senior government grants to implement sustainable utilities and services improvements.



Successful implementing the neighbourhood plan will require the efforts of many: the residents who live here, the City, developers, and neighbours.

9 | IMPLEMENTATION



The Cairnsmore Sustainable Neighbourhood Plan provides a vision, key directions, and a policy framework to guide change in the neighbourhood. This implementation section provides a summary of recommended steps for achieving the directions of this plan.

IN THIS SECTION:

- Administration and monitoring of the plan
- Phased summary tables key actions to guide next steps

9.1 ADMINISTRATION OF THE PLAN & MONITORING

Implementation of the Cairnsmore Sustainable Neighbourhood Plan will occur over time. It is important to recognize that the Cairnsmore area will continue to evolve and so it is necessary to be flexible and adapt to new opportunities and challenges that arise.

Ongoing monitoring will be required to sustain momentum and revisit directions as changes occur. It is recommended that the neighbourhood plan be reviewed periodically (every 3-5 years) and adaptive management be incorporated to consider changing circumstances, neighbourhood desires, and progress made.

Planning for successful change in Cairnsmore will be contingent on continued community engagement and dialogue. The City should work with Cairnsmore residents to keep them updated on progress.

Where City resources are limited, residents and stakeholders are encouraged to seek other means of furthering priorities in this plan, including local improvement districts, partnerships, grants, and other sources of funding.

9.2 ACTION PLAN SUMMARY

The following tables identify ongoing, short-, and medium- / long-term actions for advancing the Cairnsmore Sustainable Neighbourhood Plan. The tables identify actions, their related policies, and the roles and responsibilities anticipated in moving each action forward.

9.2.1 SHORT-TERM ACTIONS (0-5 year time frame)

Short-term actions include updates to City policy and key projects intended to catalyze positive change in the neighbourhood.

#	TOPIC	ACTION	POLICIES	ROLES
1	Sustainable Land Use, Buildings & Sites	Update the Official Community Plan to: » Incorporate the land use designations in this plan » Update Development Permit Area 1 Design Guidelines » Update Development Permit Area 4 Design Guidelines	» 4.4 Land Uses » 4.5 Neighbourhood Commercial Node » 4.6 Built Form & Site Design » 4.7 Green Buildings & Sites	» City of Duncan
2	Sustainable Land Use, Buildings & Sites	Update the Zoning Bylaw to be consistent with the directions of this plan, including: » Incorporation of heights, setbacks, and impervious area limits outlined in the Land Use Policies » Addition of a zoning option that supports the Transitional Low / Medium Density Residential land use described in this plan » Incorporation of directions to support potential for live / work units	» 4.4 Land Uses	» City of Duncan
3	Sustainable Land Use, Buildings & Sites	Develop a Community Amenity Contribution Policy that informs contributions and prioritizes the desired amenities outlined in this plan	» 4.3 Community Amenity Contributions	» City of Duncan

SHORT-TERM ACTIONS (0-5 year time frame) (Continued)

#	TOPIC	ACTION	POLICIES	ROLES
4	Sustainable Land Use, Buildings & Sites	Update development application submissions requirements to incorporate: <ul style="list-style-type: none"> » Tree Removal and Protection Plan » Solar Shading Analysis » Landscape Architectural Plans » Parking Lot Shading Analysis » Rainwater Management Plan 	<ul style="list-style-type: none"> » 4.6.2 Building Siting & Massing » 4.6.3 Site & Landscape » 4.7.3 Rainwater Management 	» City of Duncan
5	Sustainable Land Use, Buildings & Sites	Complete adoption of the BC Energy Step Code	» 4.7.1 Energy Conservation & GHG Emissions Reduction	» City of Duncan
6	Mobility	Construct a new roundabout at the Cairnsmore / Government / College intersection, including placemaking features	» 5.4 Vehicle Mobility	» City of Duncan
7	Mobility	Analyze the potential to change the Cavell and Cairnsmore intersection to a three-way stop	» 5.4 Vehicle Mobility	» City of Duncan
8	Mobility	Consider implementing reduced speed limits on Government Street and Cairnsmore Street at the neighbourhood commercial node	» 5.4 Vehicle Mobility	» City of Duncan
9	Mobility	Formalize a review process of Traffic Impact Assessments for all new major developments in the Municipality of North Cowichan that will impact traffic in the Cairnsmore neighbourhood	» 5.4 Vehicle Mobility	<ul style="list-style-type: none"> » City of Duncan » Municipality of North Cowichan
10	Mobility	Incorporate the streetscape recommendations in this plan into the City's Transportation Master Plan	<ul style="list-style-type: none"> » 5.4 Vehicle Mobility » 5.5 Street Typologies & Streetscapes 	» City of Duncan
11	Green Spaces	Update the existing tree protection bylaw and/or develop a tree planting policy that: <ul style="list-style-type: none"> » Formally incorporates tree cover targets » Identifies appropriate planting requirements including tree species and adequate soil volumes » Identifies tree replacement rates recommended in this plan 	» 6.2 Natural Areas & Urban Forest	» City of Duncan
12	Neighbourhood Well-Being	Develop a Cairnsmore neighbourhood identity and identify opportunities to implement the identity throughout the neighbourhood	» 7.4 Arts & Culture	<ul style="list-style-type: none"> » City of Duncan » Neighbourhood
13	Sustainable Utilities & Services	Incorporate the directions of the Cairnsmore Neighbourhood Plan into city infrastructure improvement planning (e.g., capital plans, development cost charges)	» 8.2 Sustainable Utilities & Services	» City of Duncan

9.2.1 ONGOING ACTIONS (as conditions permit)

Ongoing actions will be monitored and implemented based on opportunity, funding availability, and need.

#	TOPIC	ACTION	POLICIES	ROLES
1	Sustainable Land Use, Buildings & Sites	Develop landmarks at key gateway locations identified in this plan to create a welcoming entry into the Cairnsmore neighbourhood commercial node	» 4.5 Neighbourhood Commercial Node	» City of Duncan » Development Community
2	Mobility	Advance high priority active transportation improvements as funding and opportunity permits, including: » Canada Ave staircase upgrades » Jubilee Street active transportation improvements » Protected cycling facilities on Jubilee Street » Cairnsmore Street active transportation upgrades » Government Street active transportation facilities » Government Street pedestrian crosswalk improvements at Herbert Street » Improved pedestrian access across Canada Ave and the rail line to commercial and institutional destinations (e.g. Beverly Corners, VIU) » Addition of a bike maintenance facility in the neighbourhood commercial node	» 5.2 Active Transportation	» City of Duncan » Development Community
3	Mobility	Work with developers to acquire and develop the proposed lane network in the neighbourhood commercial node	» 5.4 Vehicle Mobility	» City of Duncan » Development Community
4	Mobility	Work with developers to acquire expanded road rights-of-way on Government Street and Jubilee Street	» 5.4 Vehicle Mobility	» City of Duncan » Development Community
5	Mobility	Add traffic calming measures on local residential streets as required based on resident engagement	» 5.4 Vehicle Mobility	» City of Duncan » Neighbourhood
6	Green Spaces	Work with developers to acquire and develop public park and plaza space in the neighbourhood commercial node as outlined in the policies of this plan	» 6.3 Parks, Plazas, & Open Spaces	» City of Duncan » Development Community
7	Green Spaces	Work with developers and/or property owners to secure natural park space in the northwest part of the neighbourhood	» 6.3 Parks, Plazas, & Open Spaces	» City of Duncan » Development Community » Others
8	Neighbourhood Well-Being	Collaborate with Cowichan Tribes to consider opportunities for art within the neighbourhood	» 7.4 Arts & Culture	» City of Duncan » Cowichan Tribes
9	Neighbourhood Well-Being	Incorporate public art in priority locations throughout the neighbourhood	» 7.4 Arts & Culture	» City of Duncan » Neighbourhood
10	Neighbourhood Well-Being	Support volunteers or arts organizations to plan and implement artistic improvements or events in the neighbourhood	» 7.4 Arts & Culture	» City of Duncan » Neighbourhood

9.2.2 MEDIUM / LONG-TERM ACTIONS (6-15 year time frame)

Medium / long-term actions are anticipated to be advanced in later stages of the neighbourhood plan implementation; however, their priority may be increased depending on opportunity and demand.

#	TOPIC	ACTION	POLICIES	ROLES
3	Sustainable Land Use, Buildings & Sites	Explore the benefits and challenges of adding an Established Neighbourhood Overlay and new Intensive Residential Development Permit Area, Heritage Conservation Area, or other mechanism to provide opportunity for reviewing new infill development in established low density residential areas such as Cairnsmore where character protection is desired	» 4.2 General Land Use Policies	» City of Duncan
4	Sustainable Land Use, Buildings & Sites	Work with the Municipality of North Cowichan to review municipal boundaries	» 4.2 General Land Use Policies	» City of Duncan » Municipality of North Cowichan
9	Sustainable Land Use, Buildings & Sites	Consider creating an electric vehicle policy for the City to guide appropriate allocation of electric vehicle parking and charging facilities	» 4.6.4 Access & Parking	» City of Duncan
11	Sustainable Land Use, Buildings & Sites	Develop a City-wide heritage framework	» 4.8 Built Heritage	» City of Duncan
13	Mobility	Advance moderate priority active transportation improvements as funding and opportunity permits, including: » Pedestrian facilities on College St » Active transportation improvements on Islay St » Formalization of existing informal pedestrian connections » Improved pedestrian routes and wayfinding to Centennial Park » Sidewalks on Philip St (with MNC) » Active transportation connection to the Cowichan Valley Trail (with MNC) » Active transportation connection between White Rd and Herbert St » Active transportation connection between Lake Cowichan Rd and Philip St	» 5.2 Active Transportation	» City of Duncan » Municipality of North Cowichan » Development Community
14	Mobility	Increase transit service and circulator services in Cairnsmore.\	» 5.3 Transit	» BC Transit » City of Duncan » Municipality of North Cowichan » Cowichan Valley Regional District
21	Mobility	Analyze opening of the road right-of-way from College Street to Philip Street for through traffic	» 5.4 Vehicle Mobility	» City of Duncan » Municipality of North Cowichan
27	Green Spaces	Work with SD#79 and CVOLC to improve public spaces around Duncan Elementary for both students and neighbourhood residents	» 6.3 Parks, Plazas, & Open Spaces	» City of Duncan » School District #79 » Cowichan Valley Open Learning Co-op
28	Green Spaces	Work with Girl Guides of Canada to improve public spaces around Girl Guide Hall for the neighbourhood	» 6.3 Parks, Plazas, & Open Spaces	» City of Duncan » Girl Guides of Canada
29	Green Spaces	Develop the unused portion of road right-of-way at the end of Cairnsmore Street as public park space	» 6.3 Parks, Plazas, & Open Spaces	» City of Duncan



GLOSSARY OF TERMS

- **Accessory Dwelling** - A smaller, independent residential dwelling unit located on the same lot as a detached home.
- **Adaptive Re-use** - The process of reusing a building for a purpose other than originally intended.
- **Affordable Housing** - Housing that has a rent or sale price that is no more than 30% of a household's gross annual income. Affordable housing comes in many forms including rental housing, market and non-profit housing, single family dwellings and duplexes, rental or strata suites, strata townhouses, etc.
- **Amenities** - Elements included during development that benefit the overall community or neighbourhood, for example: trails, recreational facilities, parks, etc.
- **Apartment** - A building with a number of self-contained housing units.
- **BC Energy Step Code** - A provincial standard that provides an incremental and consistent approach to achieving more energy-efficient buildings. It provides a common pathway that local governments may use to ensure BC delivers on its goal of net-zero energy-ready performance by 2032. It does so by establishing a series of measurable, performance-based energy-efficiency requirements for constructions that communities may chose to adopt when ready.
- **Bylaw** - A regulation that puts planning directions into practical and legal terms.
- **Carriage Home** - An accessory dwelling is a secondary suite located above a garage or outbuilding on the same lot as a detached home.
- **Circulation** - How pedestrians, cyclists, and vehicles flow through an area.
- **Climate Change** - A change in global or regional climate patterns, often attributed to the increased levels of atmosphere carbon dioxide produced by use of fossil fuels.

- **Cluster**- A group of dwelling units located such that each unit may not have legal frontage on a public street and more than one dwelling may exist on one lot. Clustering is used to protect a portion of a site by focusing development within a specific area.
- **Crime Prevention Through Environmental Design (CPTED)** - A philosophy and design approach used to deter criminal behaviour and increase the feeling of safety through the effective use of the built environment.
- **Demographics**- The study of population characteristics such as age, income, or household structure to help plan to meet community needs.
- **Density** - The concentration of building development allowed within a given area. Low density has less built area; high density has more.
- **Density Bonus**- An allowance of additional density in exchange for the incorporation of specified amenities in new development.
- **Design Guidelines**- Policies that provide design criteria expected for all new development to ensure the overall “character” of the development fits within its context.
- **Development Permit** - A document that includes approved site and building development plans illustrating land use, landscaping, built form, intensity of use, and appearance of the site and buildings, as well as conditions of development approval.
- **Development Permit Area (DPA)**- An area of land designated in the Official Community Plan that must get Council approval before subdividing land or constructing, adding to, or altering a building. Development Permit Areas are designated to safeguard special characteristics or site conditions.
- **Duplex** - A two-unit dwelling.
- **Environmentally Sensitive Area (ESA)**- Land or water areas containing natural features or significant ecological functions that warrant protection.
- **Form and Character**- The visible shape, structure, and qualities that are distinctive to an area in its urban design.
- **Frontage** - The width of a lot measured along the street.
- **Garden Suite** - A one-storey independent residential dwelling unit located on the same lot as a detached home.
- **Green Infrastructure** - An approach to water management that protects, restores, or mimics the natural water cycle.
- **Ground-oriented Housing** - A residential unit that has individual and direct access to the ground, whether detached or attached, including single-detached dwellings, duplexes, rowhouses, and townhouses.
- **Guidelines** - Statements of planning intent that are more detailed than policies, but not as strict as rules and regulations.
- **Heritage Conservation** - Includes, in relation to heritage, any activity undertaken to protect, preserve or enhance the heritage value or heritage character (including but not limited to character-defining elements) of heritage property or an area.
- **Heritage Designation** - Bylaw to protect a heritage property that is formally recognized for its heritage value from exterior alterations, removal or demolition without the approval of City Council.
- **Heritage Property**- A structure, building, group of buildings, district, landscape, archaeological site or other place in Canada that has been formally recognized for its heritage value.
- **Heritage Register**- A list of property that is formally recognized by the local government to have heritage value or heritage character.
- **Heritage Value** - The historic, cultural, aesthetic, scientific or educational worth or usefulness of (heritage) property or an area.
- **House Conversion**- The change of use of a building constructed as a single family dwelling or duplex, to create more housing units.
- **Infill** - New construction of a building within a predominantly developed area through redevelopment, the development of vacant or underutilized lots, or expansion or conversion of existing buildings.

- **Land Use Designation**- A classification that determines the type of future use a property can have (e.g., residential, commercial, industrial). An Official Community Plan defines land use designations.
- **Mixed Use**- Combining two or more different land uses on a single property such as multiple-family residential with commercial retail.
- **Multi-modal** - Interconnected transportation systems that support vehicles, bicycles, pedestrians, and transit.
- **Multiple-Family Residential** - A form of housing that includes several residential units on one lot such as a cluster of single detached dwellings, townhomes, triplexes, fourplexes, and apartments.
- **Natural Hazards**- Elements of the natural environment that may negatively impact a community (e.g., a landslide or forest fire).
- **Neighbourhood Plan** - A policy and planning document that outlines a specific vision and direction for a neighbourhood and provides detail on land use, circulation, parks and open space, infrastructure, amenities, and environmental protection. It is consistent with and supportive to the Official Community Plan.
- **Official Community Plan (OCP)**- A planning and land use management document that sets out the long-term vision and broad policies for a community, along with actions for getting there.
- **Open Space** - Undeveloped land that is protected from development by legislation to enhance quality of life.
- **Pedestrian-oriented** - The built environment designed to enhance pedestrian access and walkability.
- **Policy**- An official plan of action adopted within a planning document.
- **Regional Growth Strategy (RGS)**- A strategic plan that defines a region-wide vision.
- **Rowhouse**- Three or more dwelling units, located side by side and separated by common party walls.
- **Secondary Suite** - A self-contained apartment either located within a single-dwelling property or in a separate accessory building on the same property (see accessory dwelling).
- **Setbacks** - The shortest horizontal distance from a boundary of a lot to the face of the building.
- **Single-Family Residential** - A free standing residential building, generally built on a lot larger than the structure so it includes a yard.
- **Strata**- A type of housing where owners own their individual strata lots and together own the common property and assets as a strata corporation. Strata housing can come in many different forms including condos, townhomes, duplexes, or single-family homes.
- **Streetscape**- All the elements that make up the physical environment of a street and define its character, including paving, trees, lighting, building type, setbacks, pedestrian amenities, furniture, parking, etc.
- **Subdivision**- The process of dividing land into smaller parcels or consolidating parcels into a larger one.
- **Townhouse**- A variety of three or more residential buildings where individual homes lining a street share adjacent walls in common under a strata title, with each dwelling unit having a separate entrance and yard.
- **Undevelopable Area**- The portion of a lot containing utility rights-of-way or natural features which make the said portion of the lot unsuitable for the placement of buildings and structures.
- **Universal Design**- The design of environments to be usable by all people, to the greatest extent possible, without the need for adaptation or special design.
- **Zoning**- A bylaw that regulates the present use of a parcel by defining permissible density, use, siting, size, and height of buildings, and shape and dimension of lots. Zoning is a tool to implement the city's policies and land use designations set out in the OCP.

